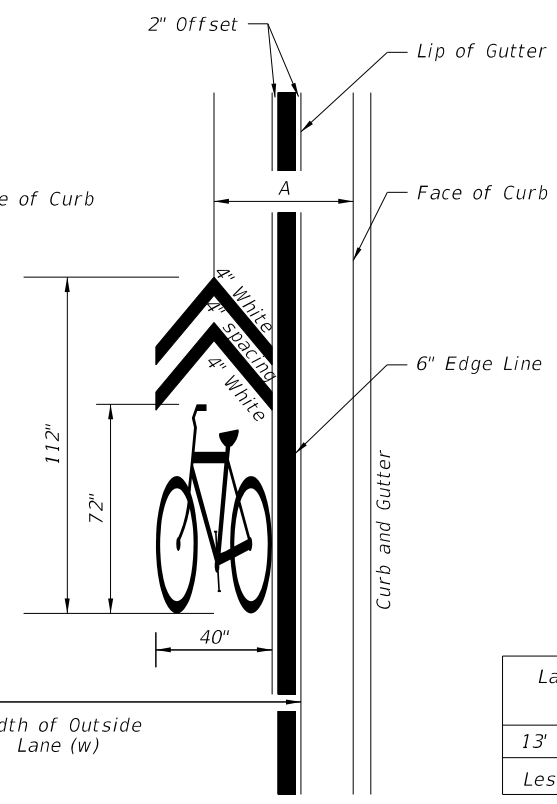
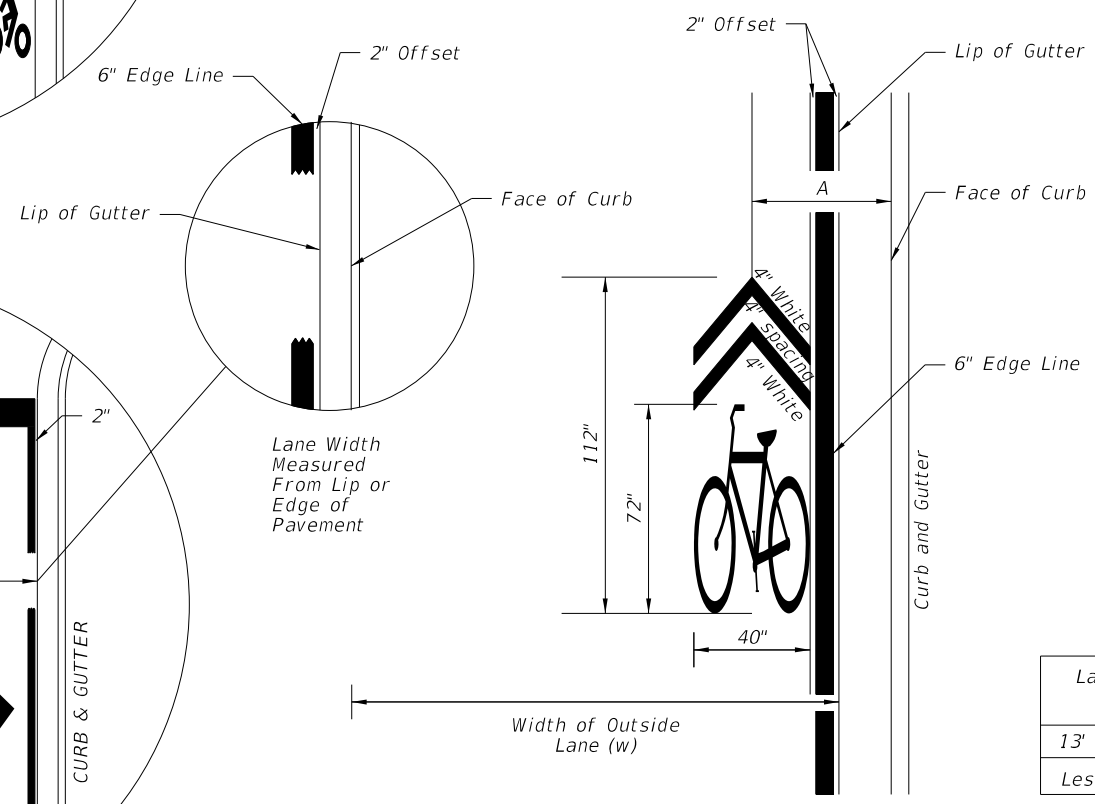
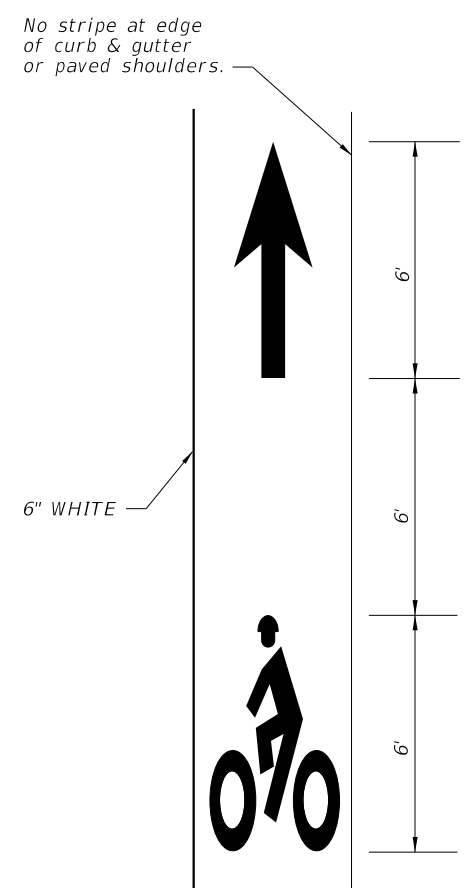
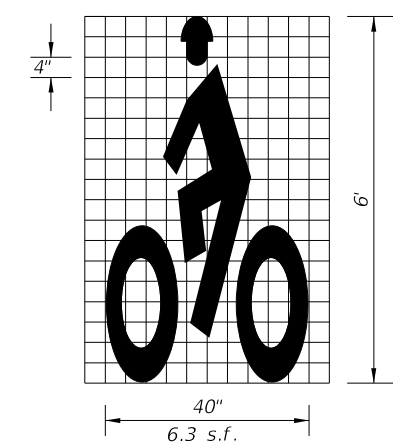
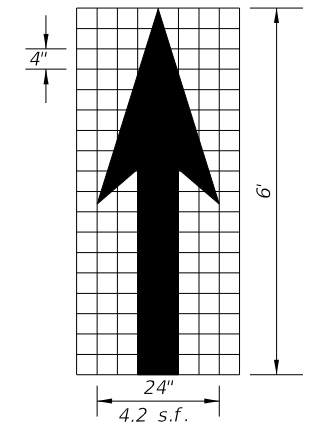
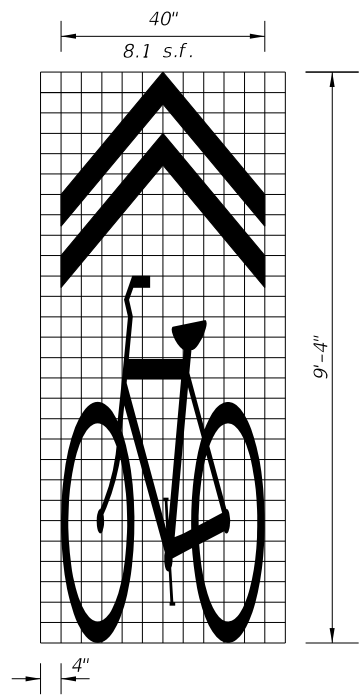
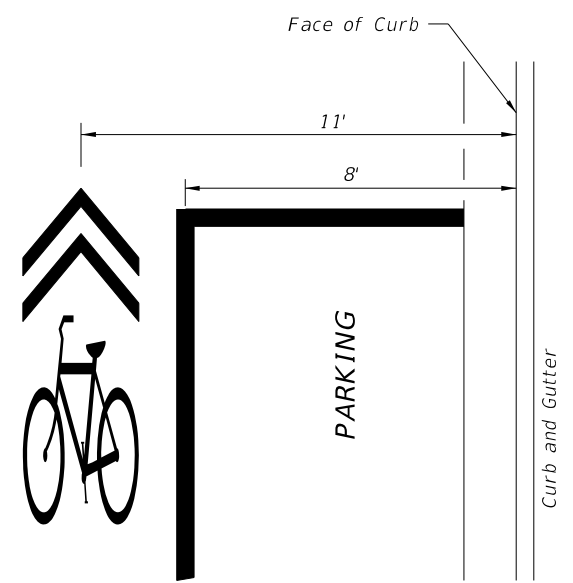
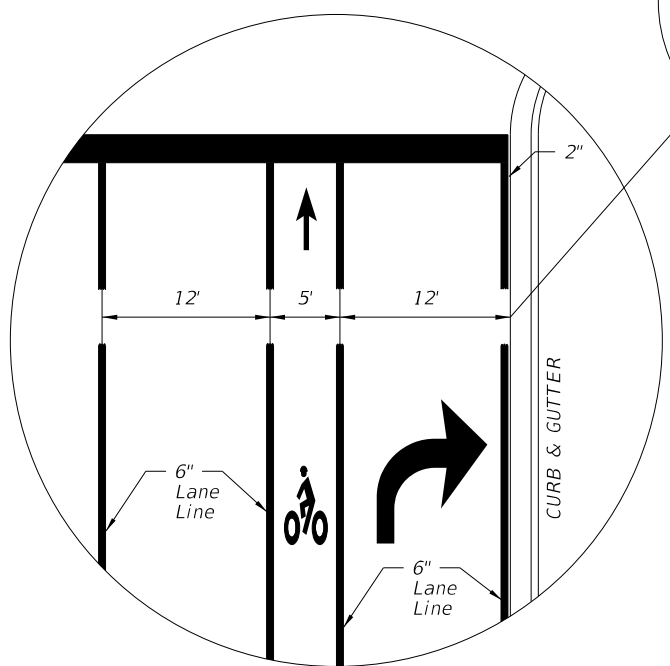
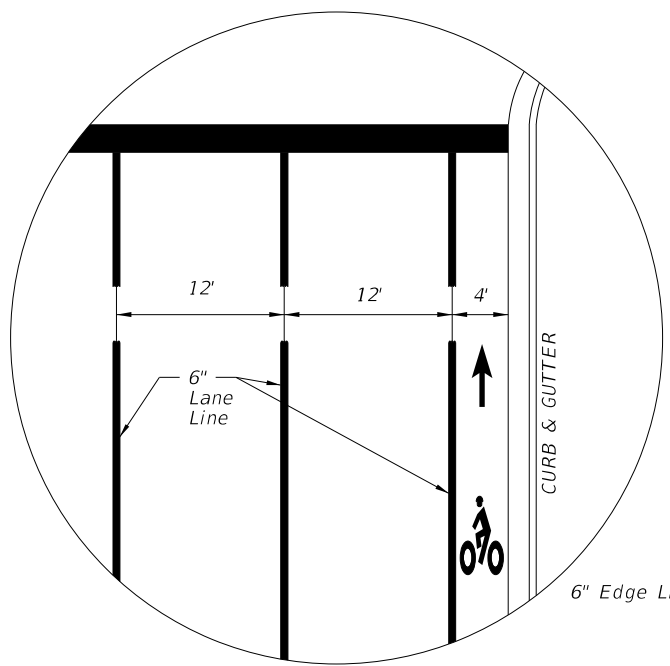
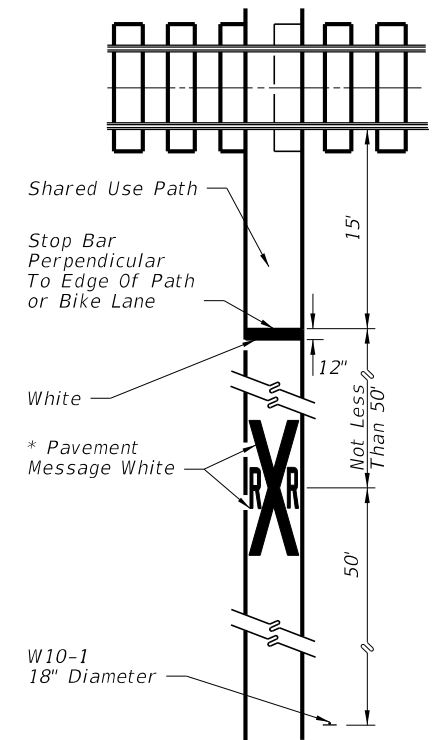
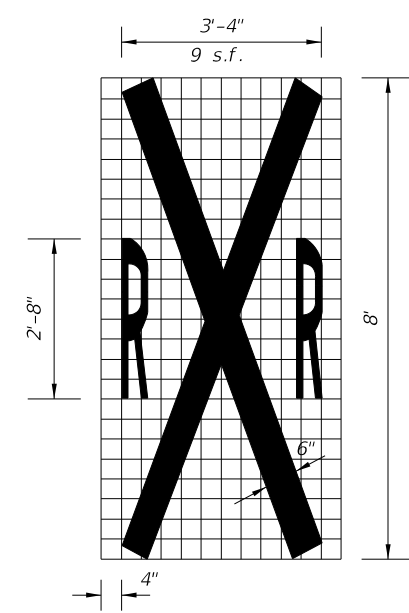


- Recommended placement of bicycle lane markings:
  - At the beginning of a bicycle lane, on the far side of major intersections, and prior to and within the bicycle lane keyhole.
  - Along the roadway as needed to provide a maximum spacing of 1,320 feet for posted speeds less than 45 mph, 2,640 feet for a posted speed of 45 mph or greater.
- Recommended spacing for shared lane marking symbols: Immediately after intersections and at a maximum spacing of 250 feet.
- All pavement markings and pavement messages shall be White.
- All pavement messages shall be preformed thermoplastic.
- Bike lane signs (R3-17, R3-17a, R3-17b) are not required.



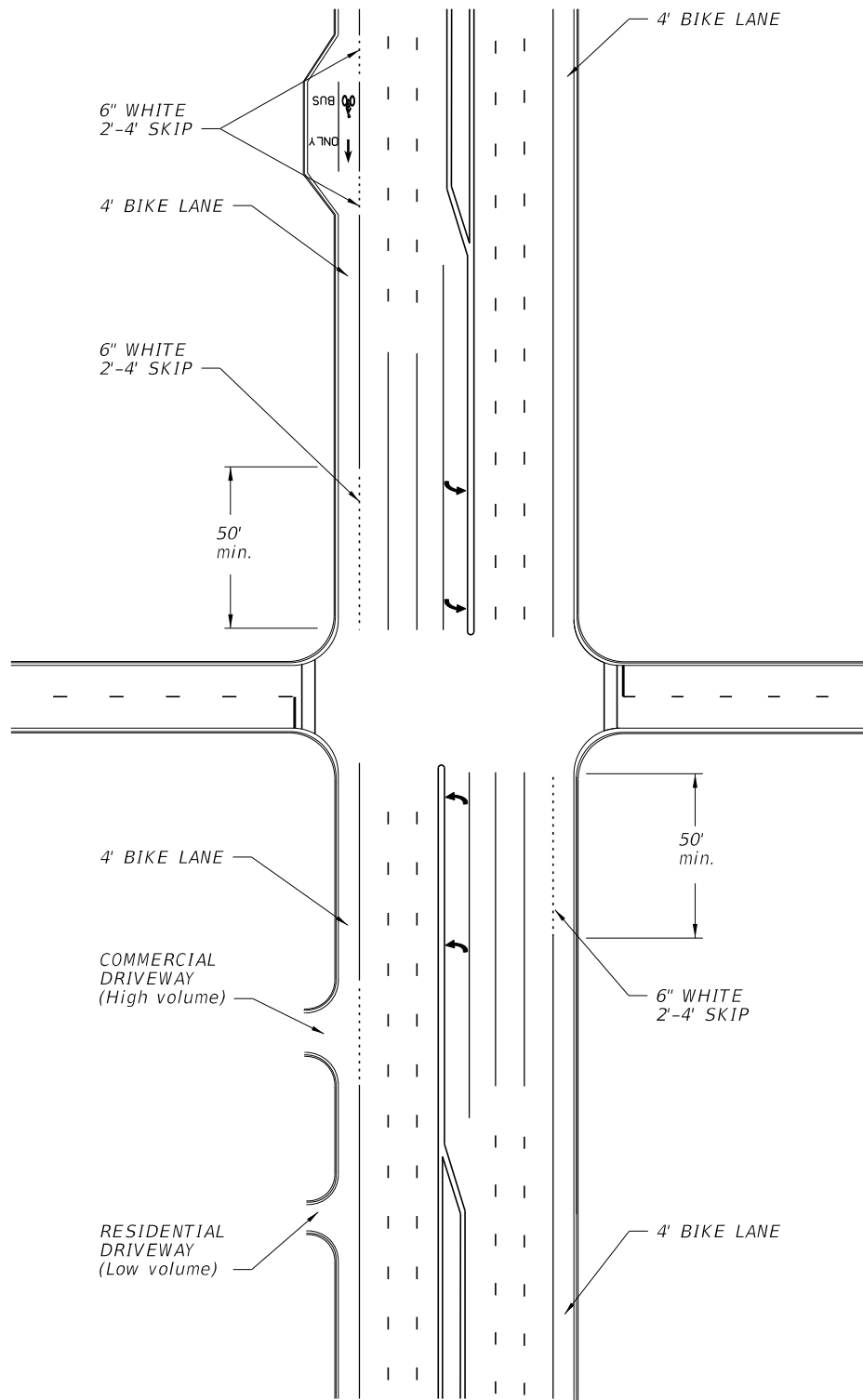
Lane Width (w)	Sharrow $\zeta$ (A)
13' or Greater	4'
Less Than 13'	5½'



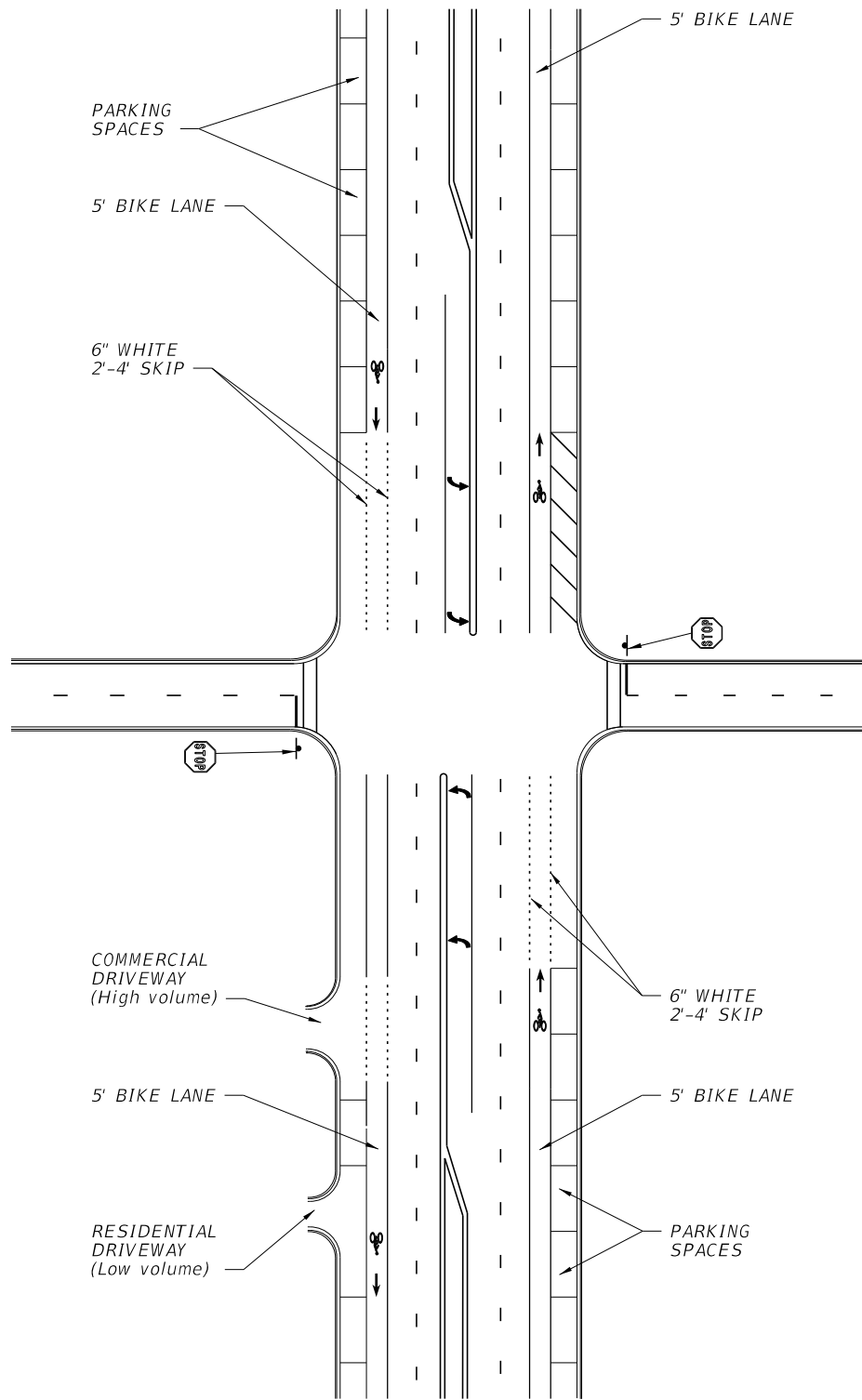
\* When used in bike lane, markings shall be placed adjacent to markings in travel lane and W10-1 sign shall be deleted.

12/30/2011 11:48:43 AM r0960/h C:\projects\standards\roadway\17300-s\17347-01.dgn

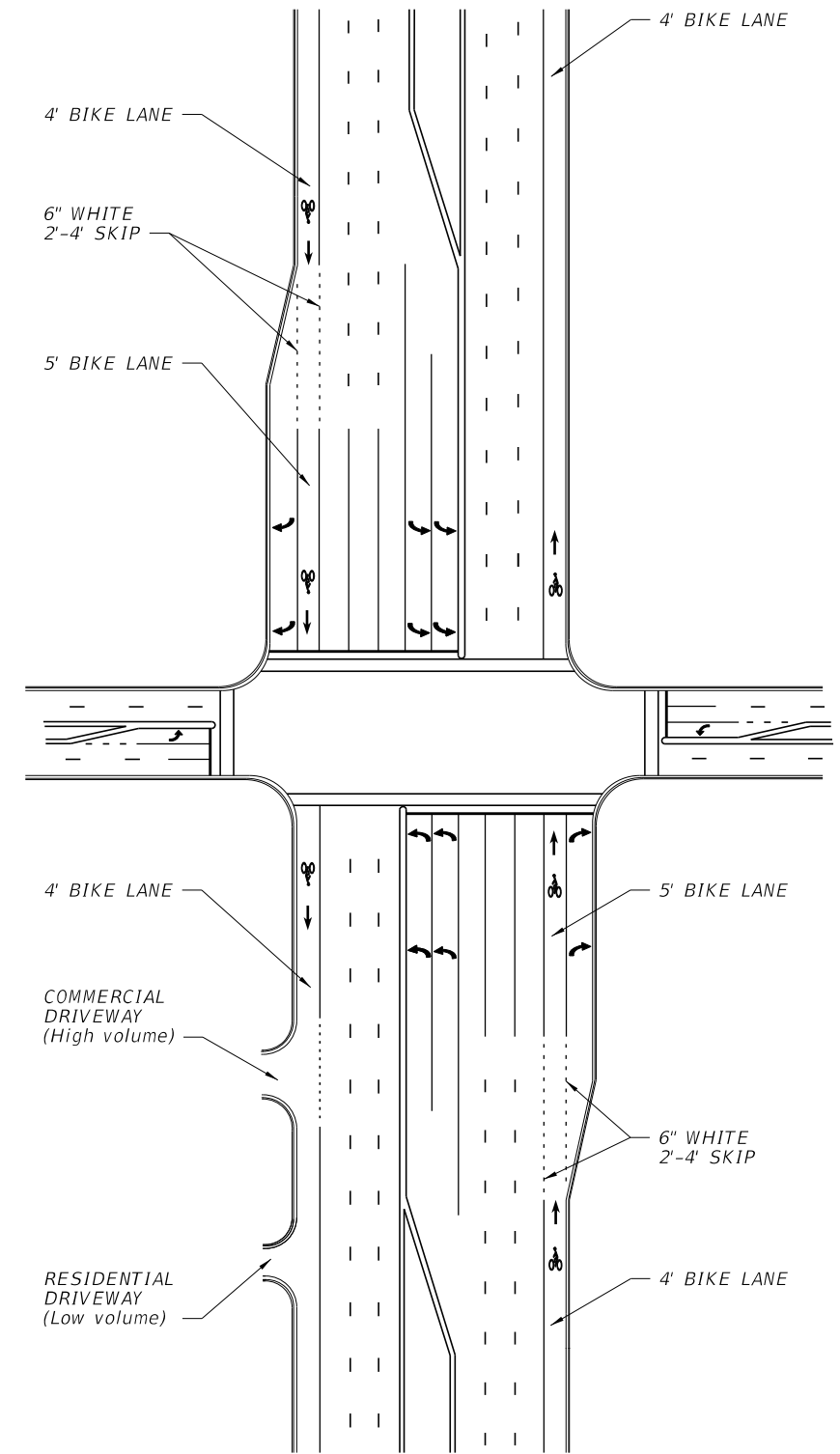
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INTERSECTION WITH BUS BAY,  
NO RIGHT TURN LANE, CURB  
AND GUTTER TYPICAL SECTION



INTERSECTION WITH ON STREET  
PARKING, NO RIGHT TURN LANE,  
CURB AND GUTTER TYPICAL SECTION



INTERSECTION WITH SEPARATE  
RIGHT TURN LANE, CURB AND  
GUTTER TYPICAL SECTION

LAST REVISION	DESCRIPTION:
01/01/12	

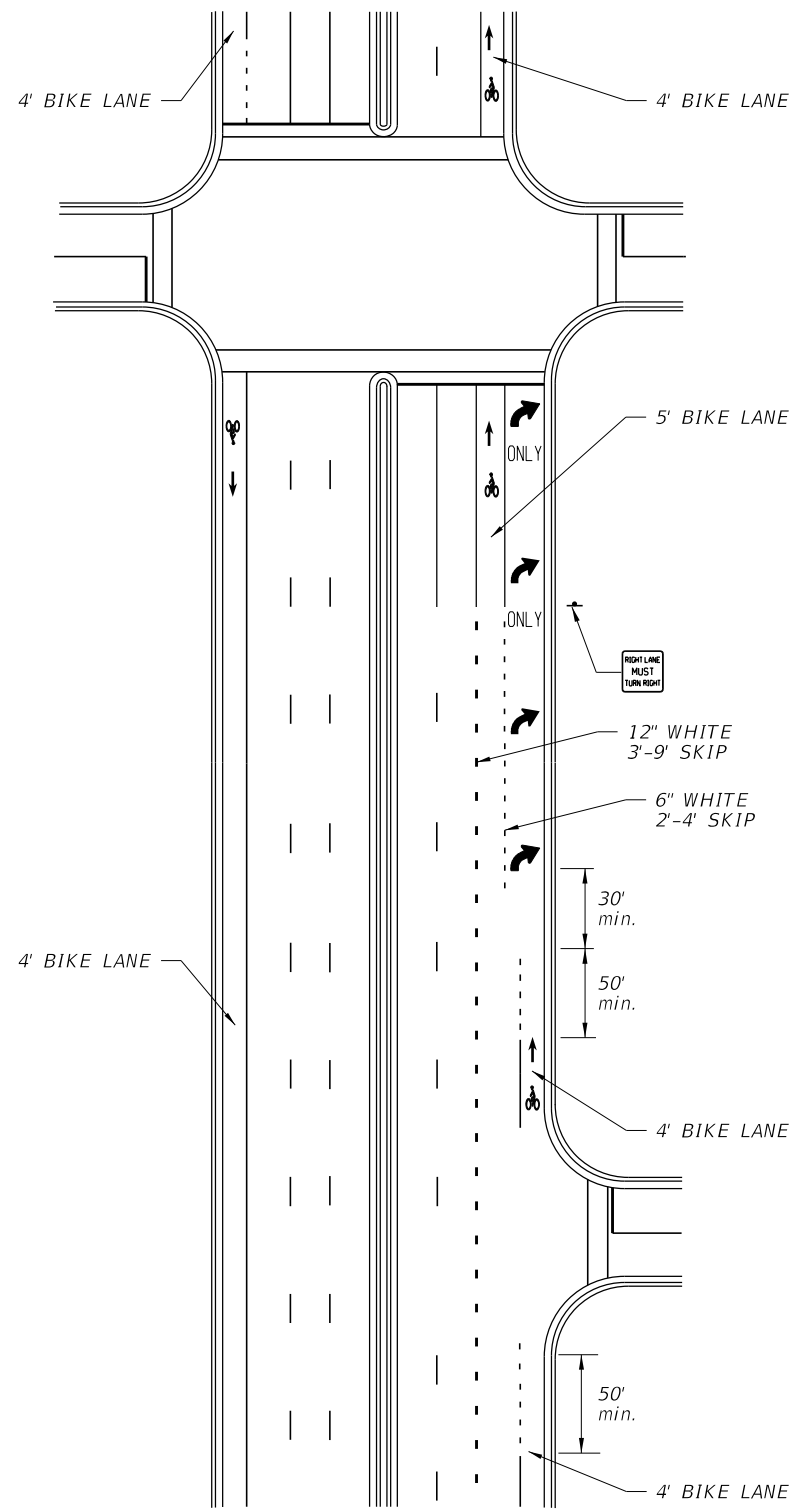


FDOT DESIGN STANDARDS  
FY 2012/2013

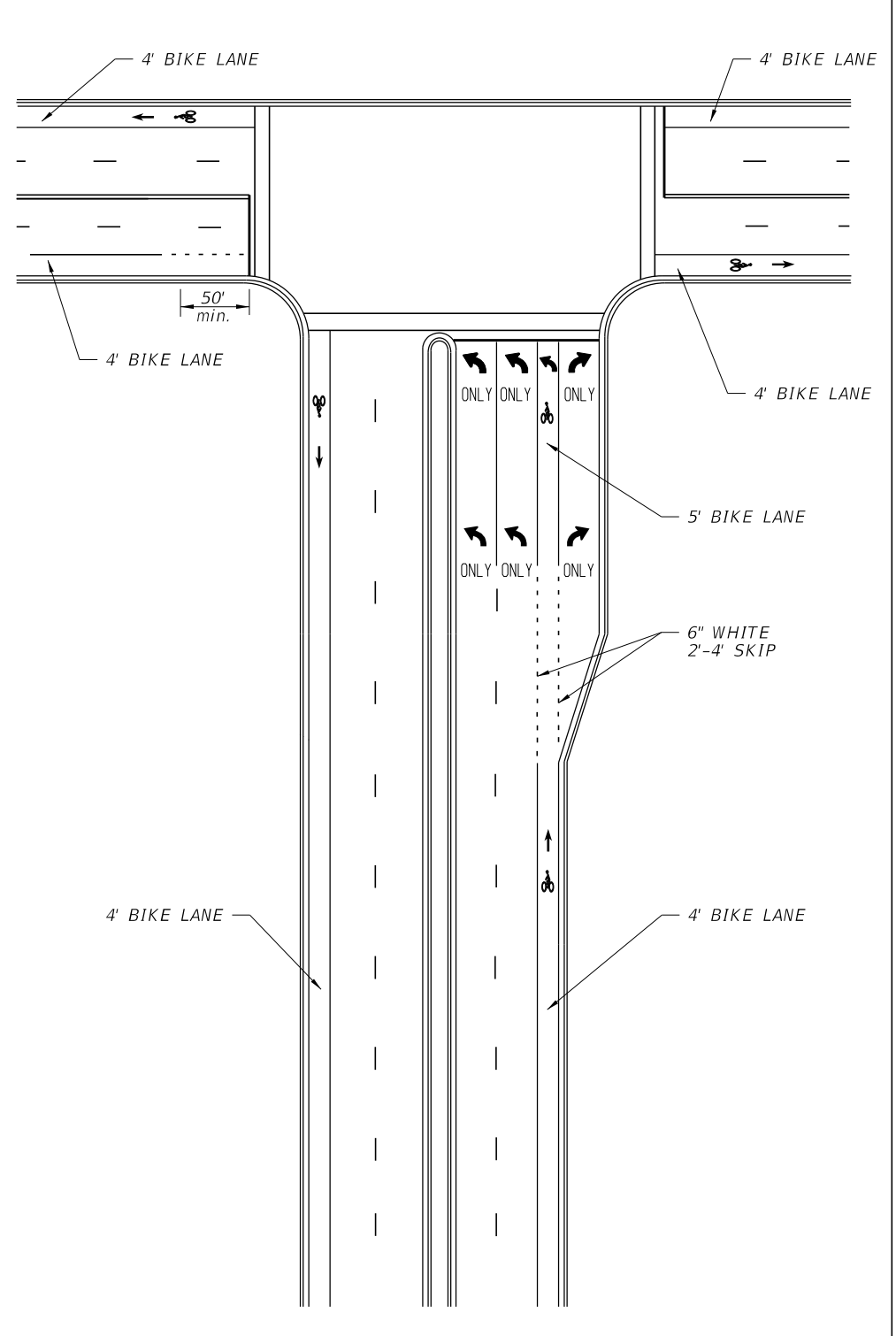
BICYCLE MARKINGS

INDEX NO.	SHEET NO.
17347	2

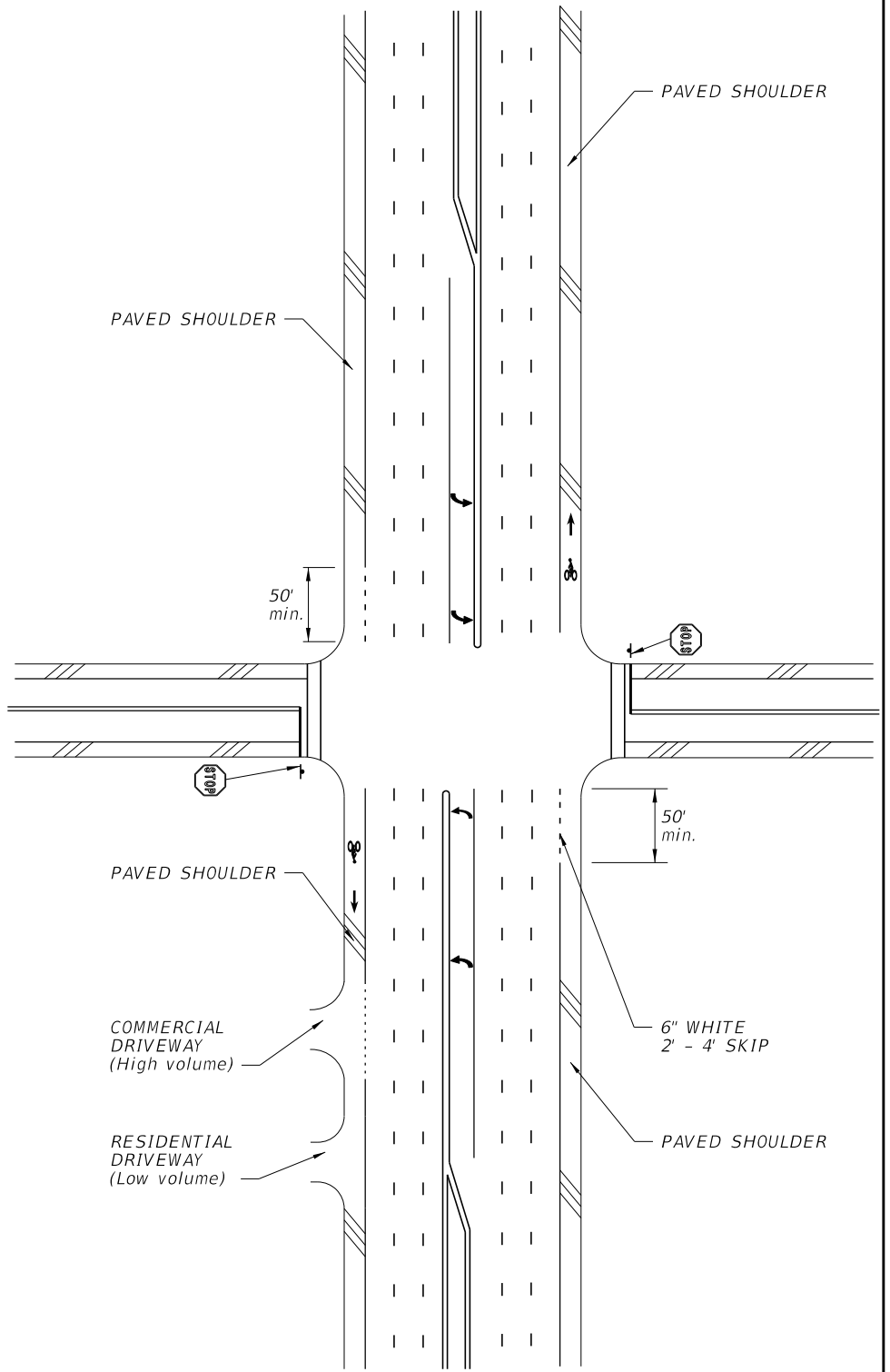
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INTERSECTION WITH RIGHT TURN DROP LANE, CURB AND GUTTER TYPICAL SECTION



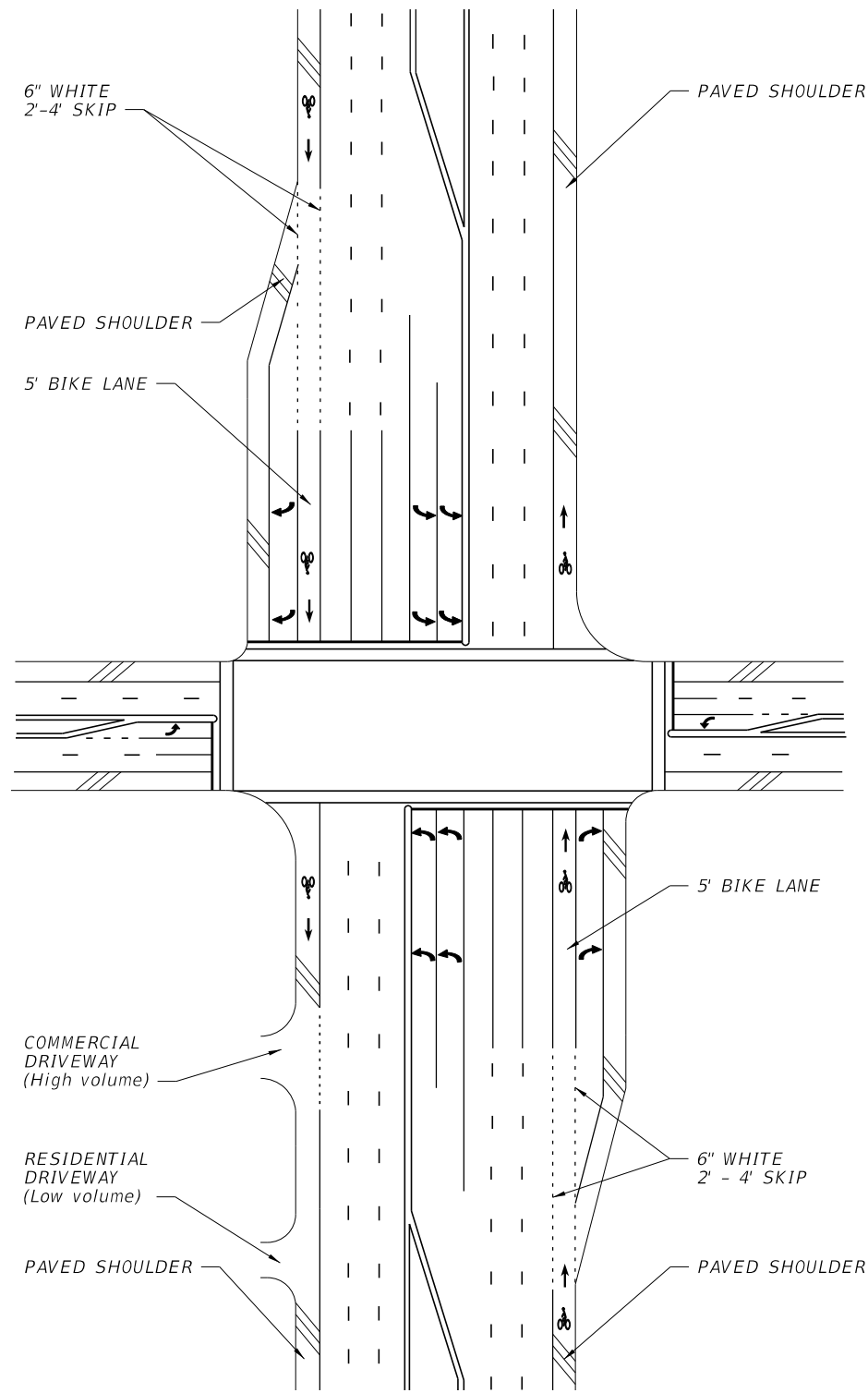
"TEE" INTERSECTION WITH SEPARATE RIGHT TURN LANE, CURB & GUTTER TYPICAL SECTION



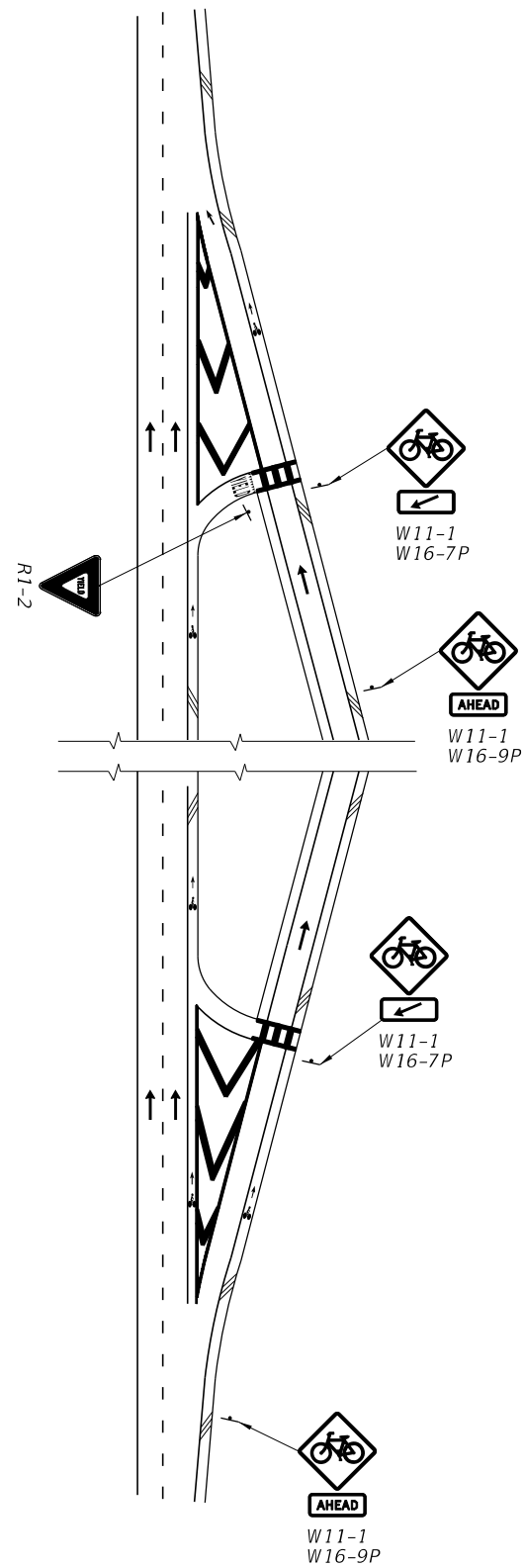
INTERSECTION, NO RIGHT TURN LANE, FLUSH SHOULDER TYPICAL SECTION

LAST REVISION	01/01/12	DESCRIPTION:	 <b>FDOT DESIGN STANDARDS</b> FY 2012/2013	<b>BICYCLE MARKINGS</b>	INDEX NO. <b>17347</b>	SHEET NO. <b>3</b>
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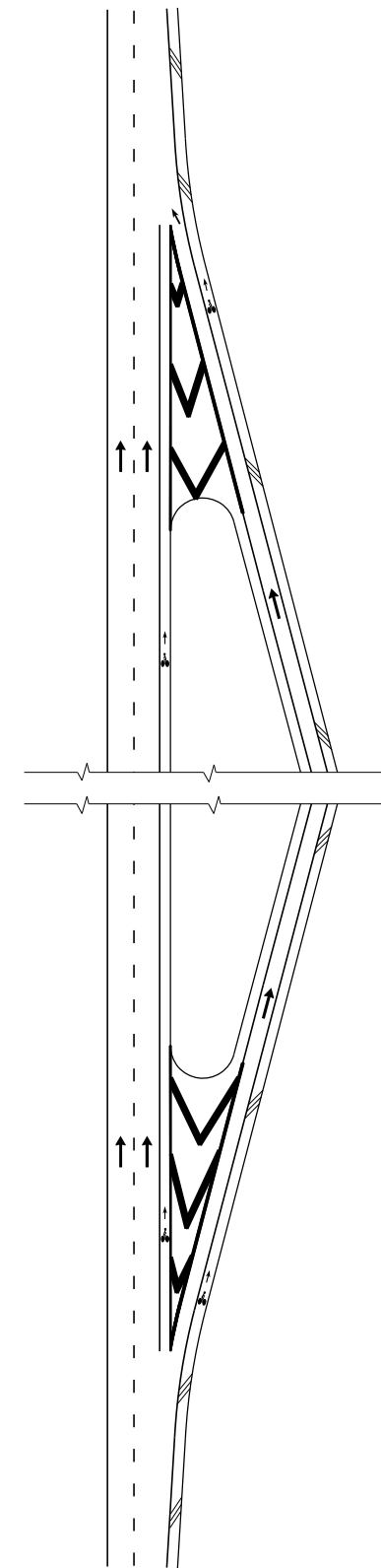
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INTERSECTION WITH SEPARATE RIGHT TURN LANE, FLUSH SHOULDER TYPICAL SECTION

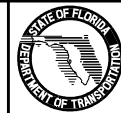


INTERCHANGE RAMP WITH MARKED CROSSING, FLUSH SHOULDER TYPICAL SECTION



INTERCHANGE RAMP, FLUSH SHOULDER TYPICAL SECTION

LAST REVISION	DESCRIPTION:
01/01/12	



FDOT DESIGN STANDARDS  
FY 2012/2013

BICYCLE MARKINGS

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17347	4