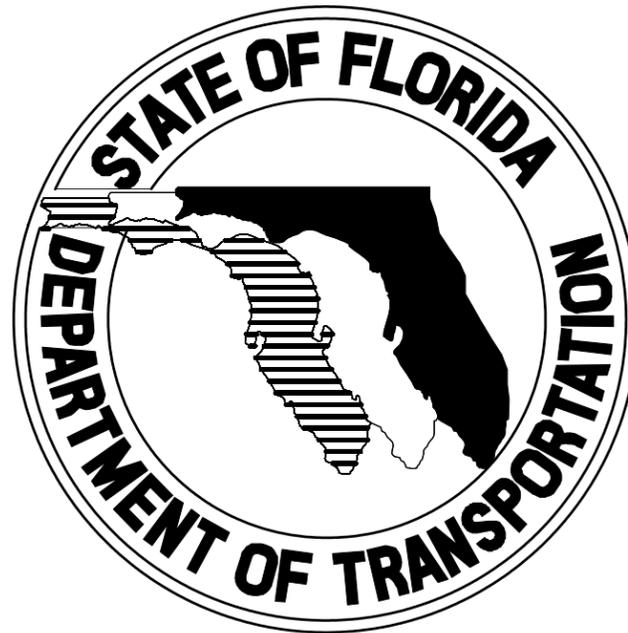
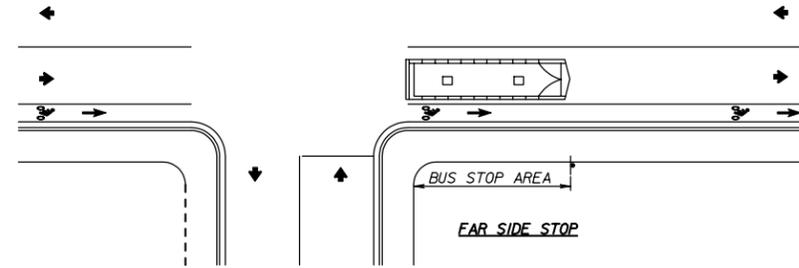
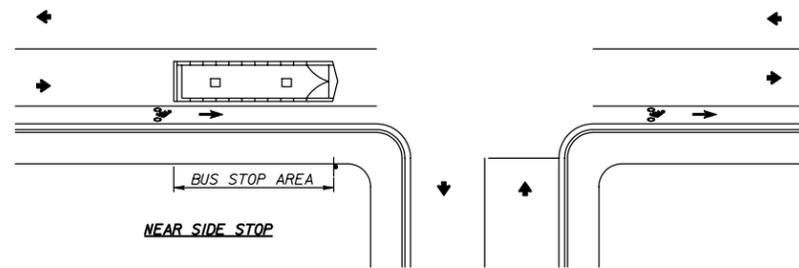


State of Florida
 Department of Transportation
 Public Transportation Office



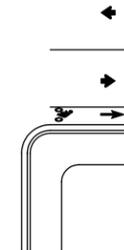
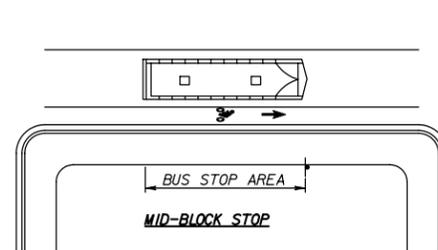
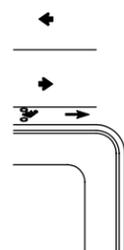
TRANSIT FACILITIES GUIDELINES

<i>REVISIONS</i>						<i>STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION</i>			<i>TRANSIT FACILITIES GUIDELINES STREETSIDE BUS STOP LOCATIONS & DESIGN TYPES</i>	<i>FIGURE</i>
<i>DATE</i>	<i>BY</i>	<i>DESCRIPTION</i>	<i>DATE</i>	<i>BY</i>	<i>DESCRIPTION</i>	<i>ROAD NO.</i>	<i>COUNTY</i>	<i>FINANCIAL PROJECT ID</i>		

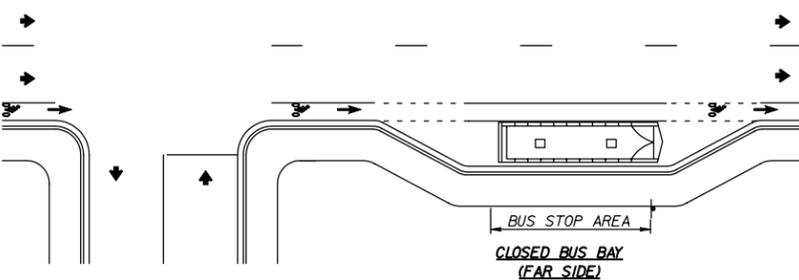
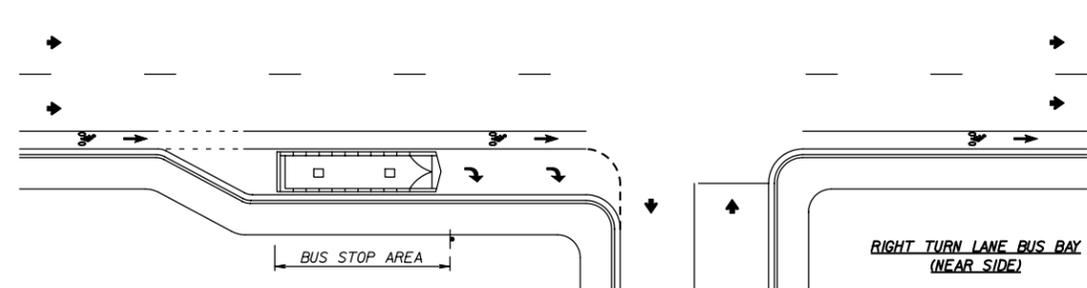
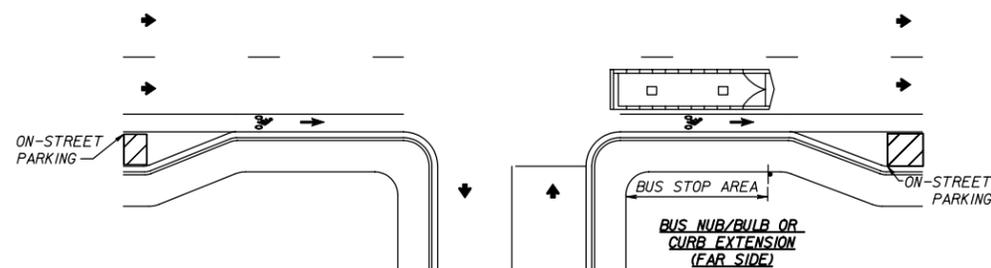
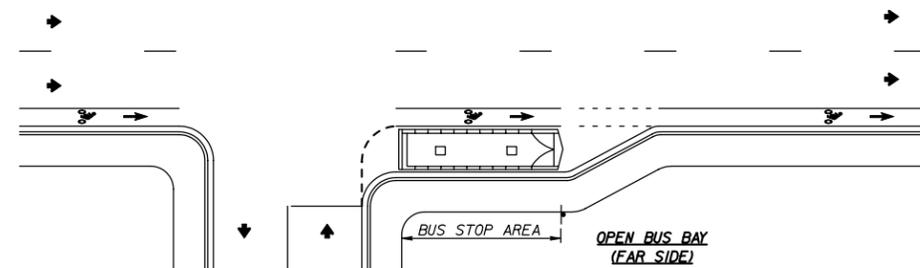
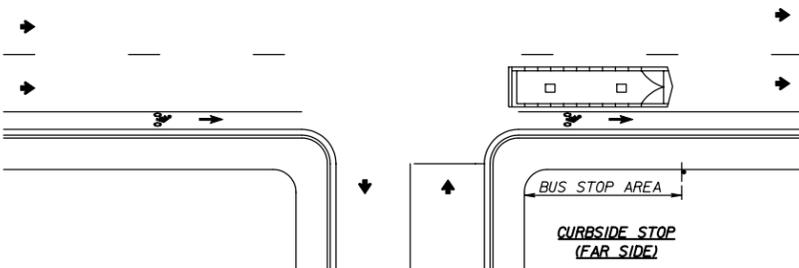


STREETSIDE BUS FACILITY GENERAL GUIDELINES (SEE ACCESSING TRANSIT HANDBOOK FOR ADDITIONAL DETAILS)

- 1) FAR SIDE BUS STOPS AND BAYS ARE GENERALLY PREFERRED OVER NEAR SIDE STOPS AND BAYS
 - EXCEPTION IS AT TWO-LANE ROADWAYS WHERE VEHICLES ARE RESTRICTED FROM GOING AROUND THE BUS STOPPED AT A CURBSIDE STOP
- 2) BUS BAYS ARE GENERALLY PREFERRED OVER CURBSIDE BUS STOPS IN TRAVEL LANES
 - PARTICULARLY FOR ARTERIAL DESIGN SPEEDS GREATER THAN OR EQUAL TO 40 MPH AND
 - WHERE THERE IS SIGNIFICANT BUS AND PASSENGER VOLUMES AND
 - WHERE PLACED DOWNSTREAM FROM A TRAFFIC SIGNAL
- 3) CLOSED BUS BAYS ARE GENERALLY PREFERRED OVER OPEN BUS BAYS
 - EXCEPTION WOULD BE AT A PHYSICALLY CONSTRAINED SITE.

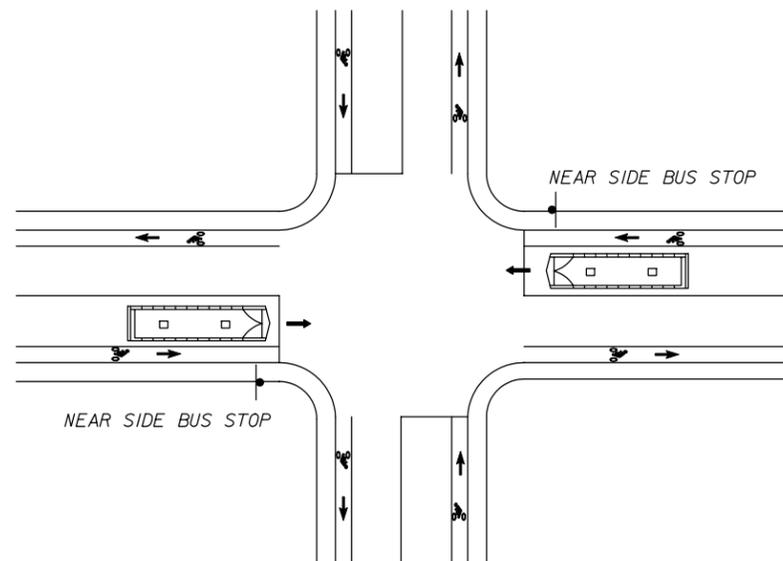
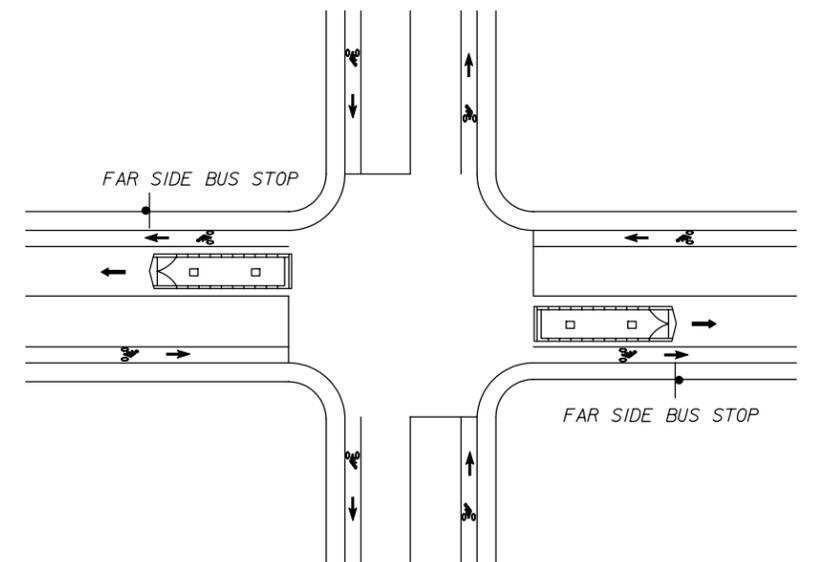
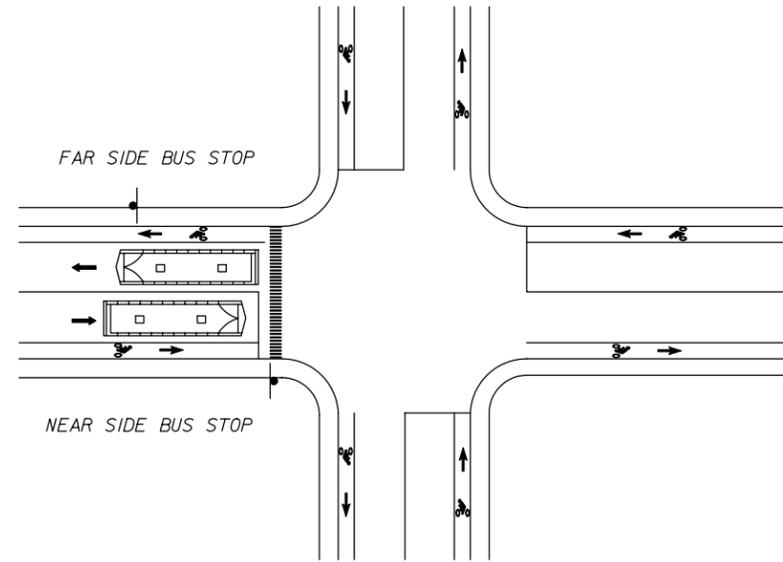
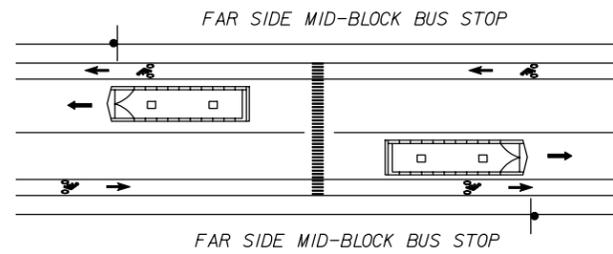
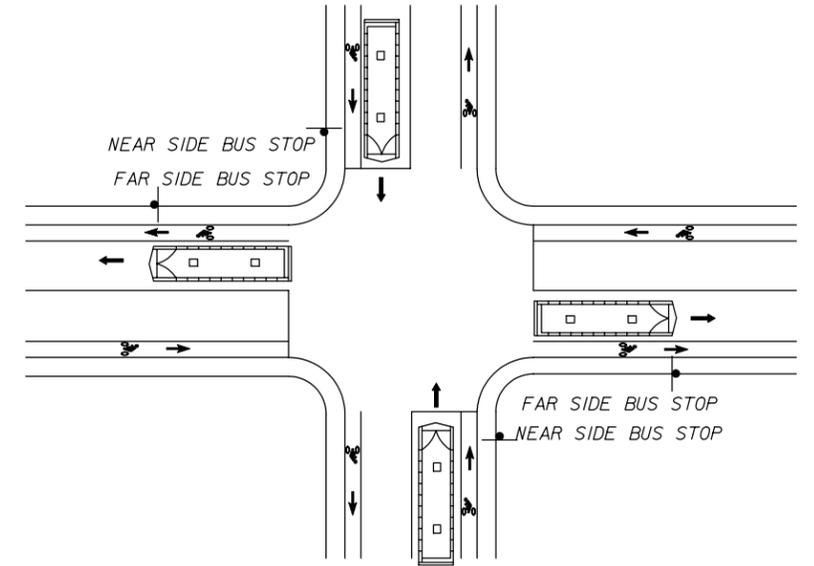
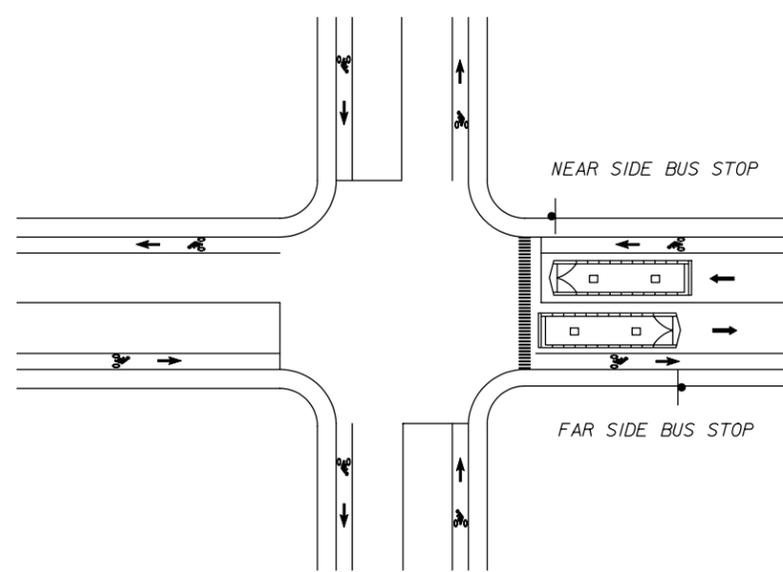
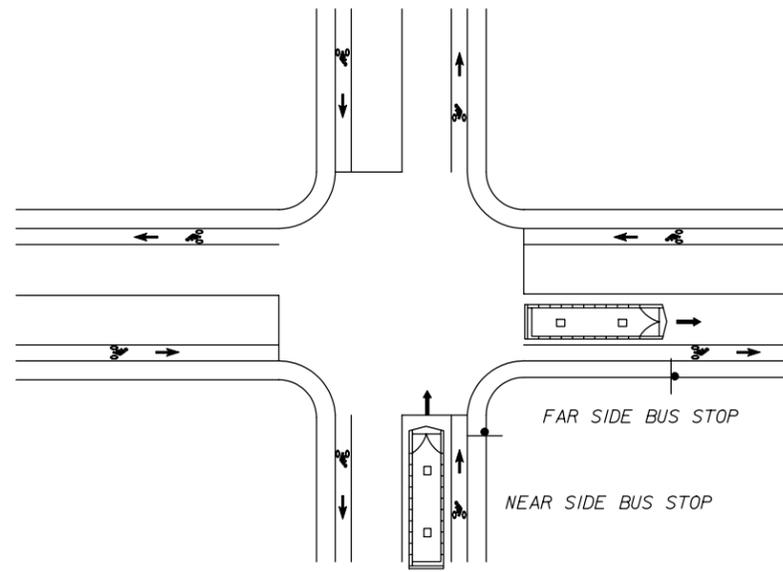


ALTERNATIVE BUS STOP LOCATIONS



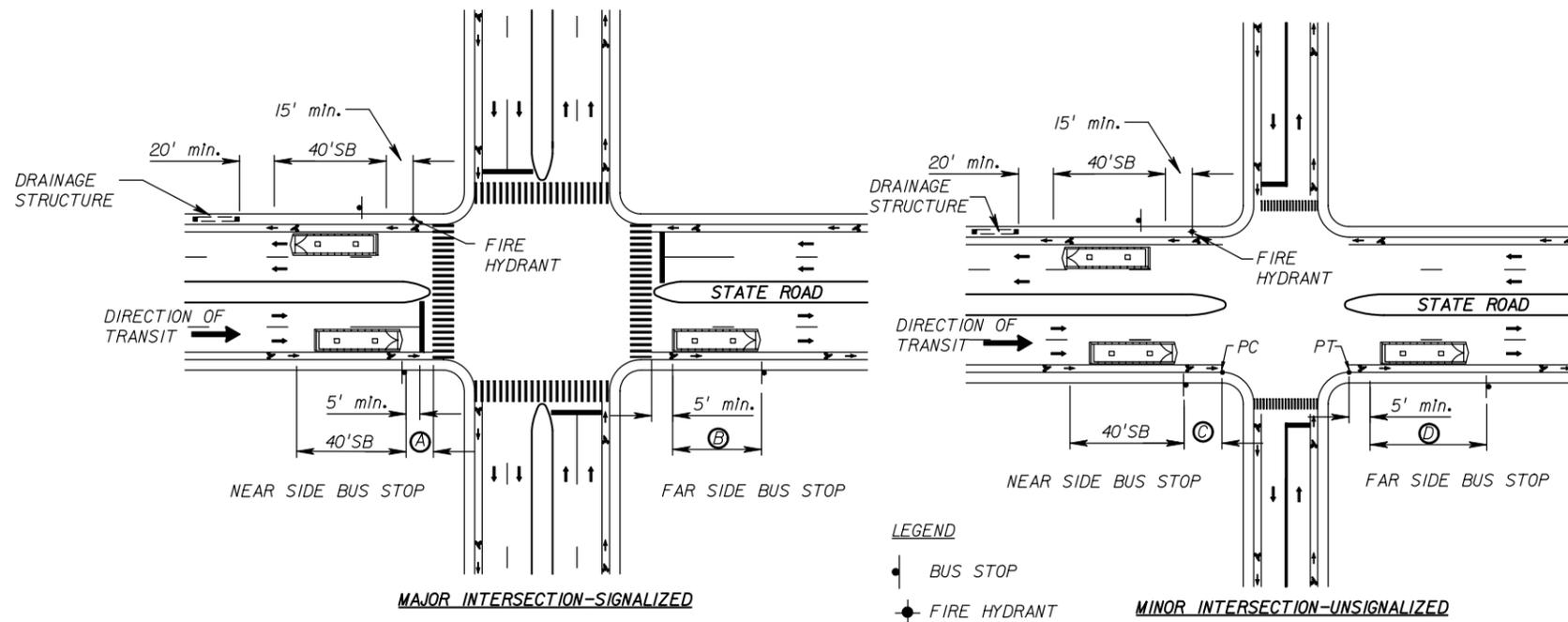
ALTERNATIVE BUS STOP DESIGN TYPES

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES STREETSIDE BUS STOP LOCATIONS & DESIGN TYPES	FIGURE 1-1
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



COMBINATION BUS STOP LOCATIONS

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES STREETSIDE COMBINATION BUS STOP LOCATIONS	FIGURE
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		1-2



GENERAL NOTES:

- THESE BUS STOP CONFIGURATIONS ARE PROVIDED AS GUIDELINES. ACTUAL BUS STOP PLACEMENT SHOULD TAKE ALL LOCATION FACTORS INTO ACCOUNT AND BE BASED ON ENGINEERING JUDGEMENT FOR SAFETY.
- ALL BUS STOPS IN URBAN AREAS (CURB & GUTTER AND SIDEWALK) SHALL BE SITUATED SO THAT PASSENGERS BOARD AND ALIGHT AT A LOCATION WHERE FULL HEIGHT CURB & GUTTER IS PRESENT AND NOT IN A SECTION OF DROP CURB.
- ALL BUS STOPS IN URBAN AREAS (CURB & GUTTER AND SIDEWALK) SHALL BE SITUATED TO BE 20' OR MORE AWAY FROM THE EDGE OF A DRAINAGE STRUCTURE, AND 15' OR MORE AWAY FROM A FIRE HYDRANT OR DISABLED PARKING SPACE.
- WHEN POSSIBLE, PROVIDE BUS STOP AT AN EXISTING ROADWAY LIGHT POLE. A 4' CLEAR TRAVEL PATH MUST BE PROVIDED ON SIDEWALKS AT ALL TIMES IN THE VICINITY OF THE BUS STOP. THE RECOMMENDED MINIMUM ILLUMINATION LEVEL IS 1.0 HORIZONTAL FOOT CANDLE FOR THE ENTIRE BUS STOP AREA. (REFERENCE PLANS PREPARATION MANUAL).
- FOR A.D.A. LANDING PAD REQUIREMENTS SEE FIGURE 1-5.
- ALL BUS BAY DRAWINGS REPRESENT A BUS STOP WITH SPACE FOR ONE BUS AT A TIME. IF MORE THAN ONE BUS IS EXPECTED, ADD 40' FOR EACH ADDITIONAL STANDARD BUS AND 60' FOR EACH ADDITIONAL ARTICULATED BUS.
- DECELERATION/ACCELERATION LANE REQUIREMENTS SHOULD BE CONSIDERED FOR BUS BAY TYPE DESIGNS IF RIGHT OF WAY ALLOWS.

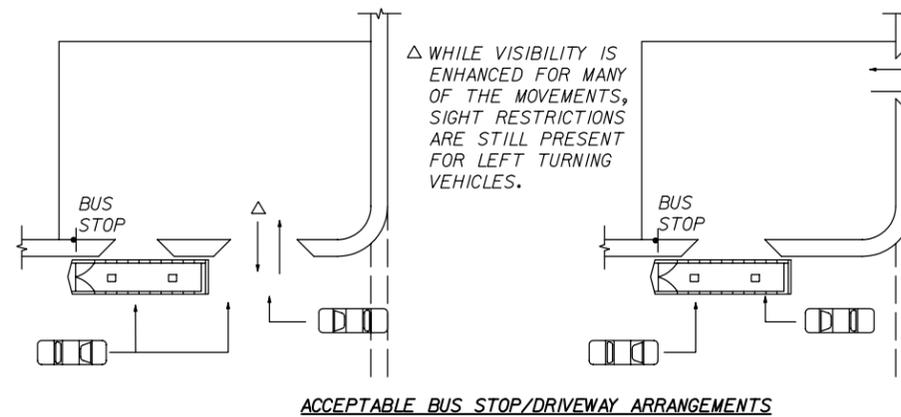
BUS STOP VARIABLES	NO TURN LANES IN DIRECTION OF TRANSIT	RIGHT TURN LANE ON NEAR SIDE IN DIRECTION OF TRANSIT	RIGHT TURN LANE ON NEAR SIDE AND AUXILIARY LANE ON FAR SIDE IN DIRECTION OF TRANSIT **	AUXILIARY LANE ON FAR SIDE IN DIRECTION OF TRANSIT ***
DIMENSION (A) (NEAR SIDE MAJOR INTERSECTION)	NOT RECOMMENDED * UNLESS 2-LANE ROADWAY	NOT RECOMMENDED UNLESS 2-LANE ROADWAY 10' BEFORE ENTRY TAPER FOR TURN BAY 100' IF DROP LANE	NOT RECOMMENDED UNLESS 2-LANE ROADWAY	NOT RECOMMENDED * UNLESS 2-LANE ROADWAY
DIMENSION (B) (FAR SIDE MAJOR INTERSECTION)	40' FOR A STANDARD BUS (60' FOR AN ARTICULATED BUS)	40' FOR A STANDARD BUS (60' FOR AN ARTICULATED BUS)	110' **	45' FOR A STANDARD BUS (65' FOR AN ARTICULATED BUS) ***
DIMENSION (C) (NEAR SIDE MINOR INTERSECTION)	12' MIN.	AS CLOSE TO ENTRY TAPER AS FEASIBLE	N/A	N/A
DIMENSION (D) (FAR SIDE MINOR INTERSECTION)	40' FOR A STANDARD BUS (60' FOR AN ARTICULATED BUS)	40' FOR A STANDARD BUS (60' FOR AN ARTICULATED BUS)	N/A	N/A

- * IF NECESSARY, 12' MINIMUM DIMENSION IS REQUIRED.
- ** THIS COMBINATION OF BUS BAYS IS REFERRED TO AS A QUEUE BYPASS BUS BAY.
- *** THIS ARRANGEMENT IS REFERRED TO AS AN OPEN BUS BAY.

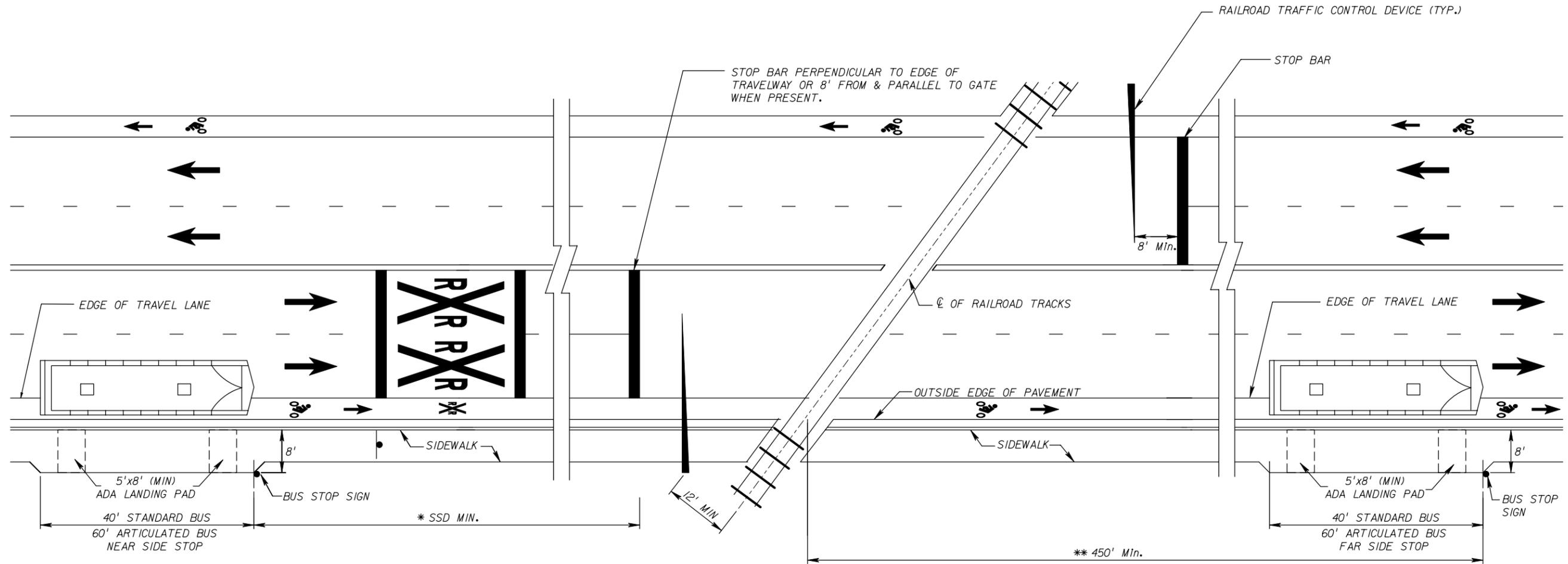
NOTE: THIS TABLE IS PREPARED AS A GUIDELINE FOR THE LOCATION OF BUS STOPS ALONG A STATE ROAD FACILITY WHERE ON-STREET PARKING DOES NOT EXIST.

DRIVEWAY BUS STOP LOCATION GUIDELINES:

- AVOID BUS STOPS THAT BLOCK THE DRIVEWAY OF A PARCEL WITH A SINGLE DRIVEWAY.
- BUS STOPS SHOULD NOT BE LOCATED WITHIN THE AREA OF INFLUENCE OF A DRIVEWAY TO AVOID SIGHT DISTANCE AND OTHER CONFLICTS. HOWEVER, IF THE SITUATION CANNOT BE AVOIDED:
- LOCATE THE STOP AS FAR DOWNSTREAM (FAR SIDE) FROM THE DRIVEWAY AS FEASIBLE.
- AVOID UPSTREAM (NEAR SIDE) STOPS IN THE TRAVEL LANE. UPSTREAM BAYS ARE ACCEPTABLE.
- LOCATE THE STOP TO ALLOW APPROPRIATE VISIBILITY FOR VEHICLES ENTERING OR LEAVING THE DRIVEWAY AND TO MINIMIZE VEHICLE/BUS CONFLICTS.
- LOCATE THE STOP SO THAT PASSENGERS ARE NOT FORCED TO WAIT FOR A BUS IN THE MIDDLE OF A DRIVEWAY.
- LOCATE THE STOP SO THAT PATRONS BOARD OR ALIGHT DIRECTLY FROM THE CURB RATHER THAN FROM THE DRIVEWAY.
- LOCATE THE STOP SO THAT THE FRONT DOOR ADA LANDING PAD IS LOCATED OUTSIDE THE DRIVEWAY.
- ATTEMPT TO KEEP AT LEAST ONE EXIT AND ENTRANCE LANE OPEN FOR VEHICLES ACCESSING A DRIVEWAY WHILE A BUS IS LOADING OR UNLOADING PASSENGERS.
- EVALUATE RELOCATING A BUS STOP TO A DOWNSTREAM PARCEL SHOULD A CORNER LOCATION PROVE TO BE UNACCEPTABLE.



REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION RELATIVE TO ACCESS POINTS	FIGURE 1-3
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



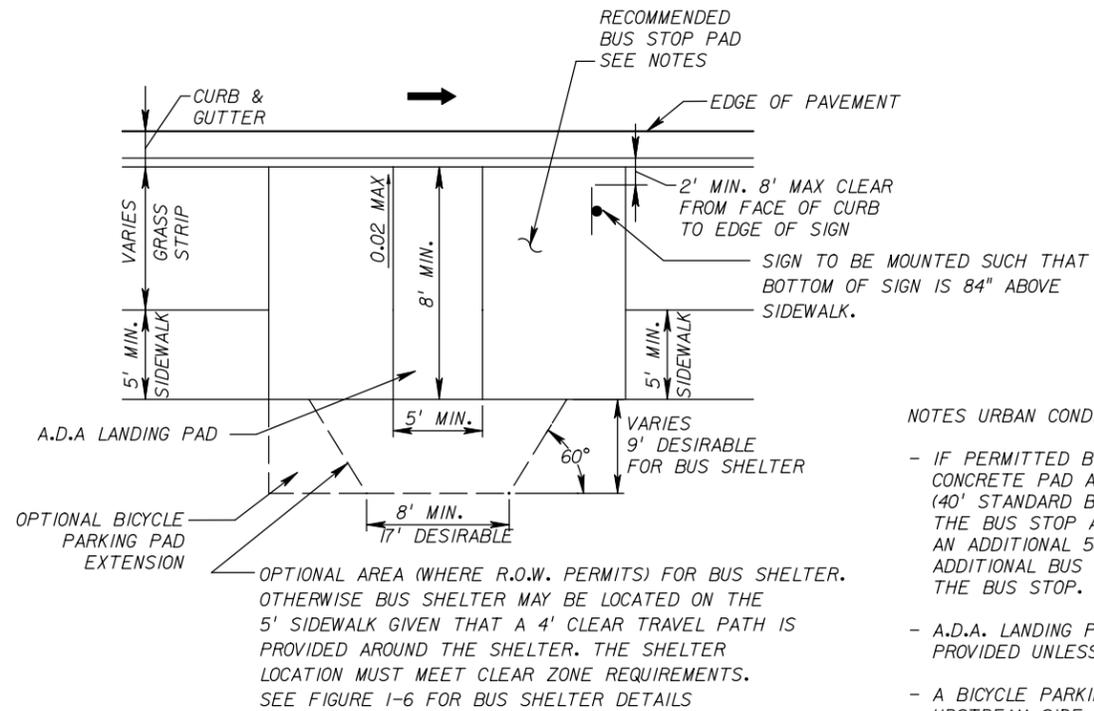
URBAN BUS STOP NEAR AT-GRADE RAILROAD CROSSING

NEAR SIDE BUS STOP PLACEMENT	
DESIGN SPEED (MPH)	* AASHTO STOPPING SIGHT DISTANCE (SSD) (FT)
25	155
30	200
35	250
40	305
45	360
50	425
55	495
60	570
65	645
70	730

NOTES:

- WHEN POSSIBLE IT IS RECOMMENDED TO PLACE BUS STOPS ON THE NEAR SIDE OF A RAILROAD CROSSING TO AVOID CREATING A QUEUE THAT WOULD CONFLICT WITH THE CROSSING.
- NEAR SIDE BUS STOPS SHALL BE LOCATED SO THAT RAILROAD WARNING SIGNS ARE NOT OBSTRUCTED BY A STOPPED BUS.
- SEE FDOT STD INDEX 17346 AND 17882 FOR RAILROAD MARKING AND SIGN DETAILS NOT SHOWN HERE.
- FOR NEAR SIDE OR FAR SIDE BUS BAYS, PROVIDE A MINIMUM OF 50 FT TO THE NEAREST RAIL LINE. (PER FLORIDA STATUTE 316.1945(1))
- FOR ADA LANDING PAD REQUIREMENTS SEE FIGURE 1-5.
- * MIN VALUE CALCULATED BASED ON PROVIDING APPROPRIATE STOPPING SIGHT DISTANCE TO RAILROAD CROSSING. DIMENSION SHOULD BE VERIFIED BASED ON THE SPECIFIC DESIGN CONDITIONS OF EACH CROSSING.
- ** MIN VALUE CALCULATED BASED ON ACCOMMODATING THE QUEUE THAT WOULD DEVELOP DURING A 60 SECOND PERIOD BUS STOP. DIMENSION SHOULD BE VERIFIED BASED ON THE SPECIFIC DESIGN CONDITIONS, TRAFFIC PROJECTIONS, AND EXPECTED BUS STOP DELAYS AT EACH CROSSING.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION RELATIVE TO RAILROAD CROSSING	FIGURE 1-4
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

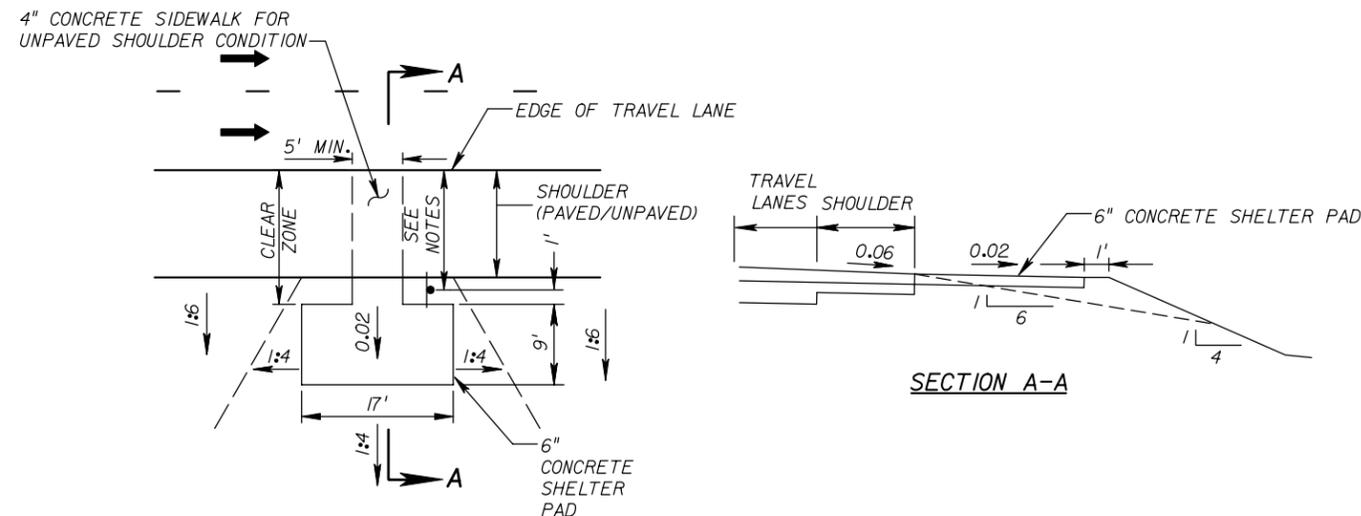


**BUS STOP PAD AND SIGNAGE
URBAN/CURB & GUTTER CONDITION WITH GRASS STRIP**



**BUS STOP PAD AND SIGNAGE
URBAN/CURB & GUTTER CONDITION WITHOUT GRASS STRIP**

- NOTES URBAN CONDITION (WITH / WITHOUT GRASS STRIP):
- IF PERMITTED BY R.O.W., PROVIDE A CONTINUOUS 8' WIDE CONCRETE PAD ALONG THE ENTIRE LENGTH (40' STANDARD BUS - 60' ARTICULATED BUS) OF THE BUS STOP ADJACENT TO THE CURB & GUTTER. AN ADDITIONAL 50' LENGTH IS RECOMMENDED FOR EACH ADDITIONAL BUS EXPECTED TO SIMULTANEOUSLY STOP AT THE BUS STOP.
 - A.D.A. LANDING PAD MINIMUM DIMENSIONS ARE TO BE PROVIDED UNLESS R.O.W. DOES NOT ALLOW.
 - A BICYCLE PARKING AREA SHOULD BE PROVIDED ON THE UPSTREAM SIDE OF THE BUS SHELTER PAD BASED ON COORDINATION WITH THE LOCAL TRANSIT AGENCY.

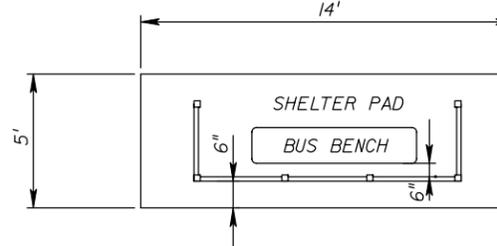
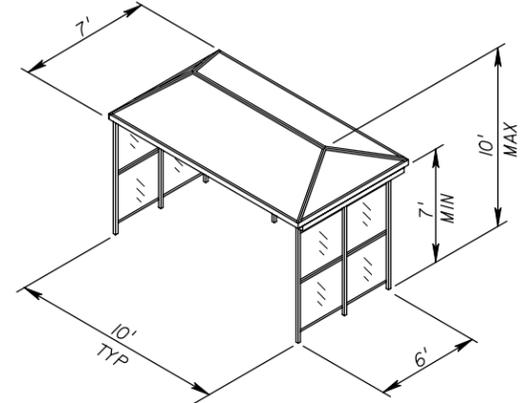
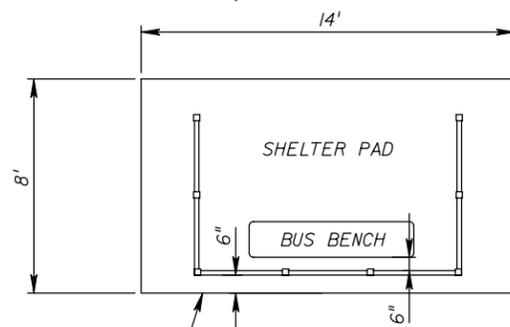
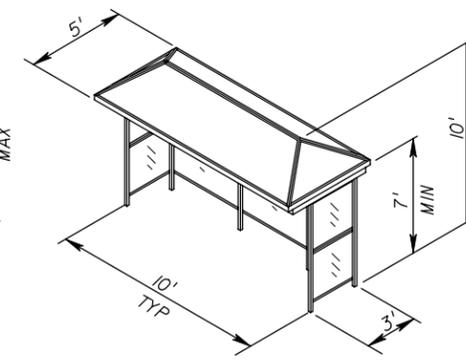
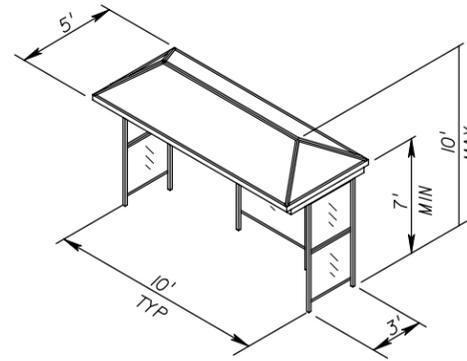
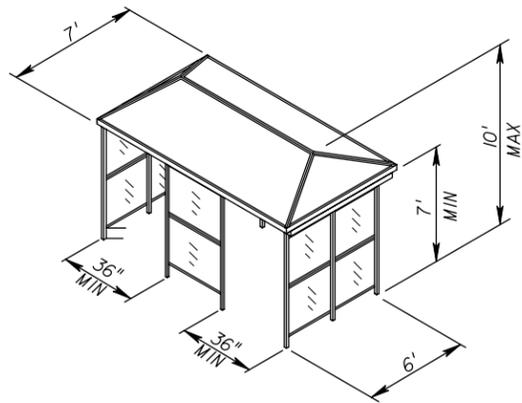
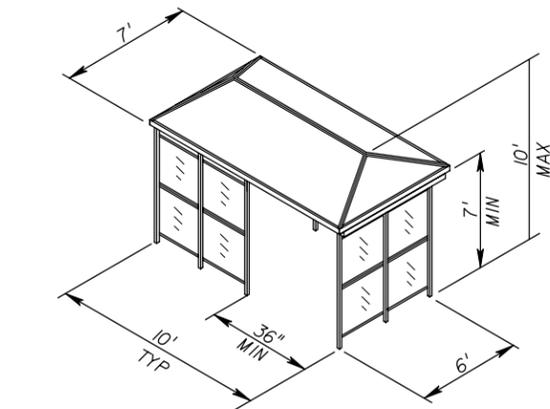


- NOTES RURAL CONDITION:
- FOR A BUS STOP IN RURAL CONDITIONS, THE BUS STOP SIGN SHOULD BE LOCATED OUTSIDE THE CLEAR ZONE OF THE ROAD; WHERE POSSIBLE IT IS DESIRABLE TO BUILD A CONCRETE PAD FOR A SHELTER.
 - THE CONCRETE PAD SHOULD BE LOCATED OUTSIDE THE CLEAR ZONE. CONCRETE SIDEWALK WITHIN CLEAR ZONE SHOULD MEET ADA CRITERIA.
 - SIZE OF THE BUS SHELTER PAD IS ADJUSTABLE DEPENDING ON LOCAL CONDITIONS. COORDINATE SIZE WITH THE LOCAL TRANSIT AGENCY.
 - SLOPES ARE REPRESENTATIVE AND VARY WITH LOCATION. ADJUST DESIGN TO LOCAL CONDITIONS.

**BUS STOP PAD AND SIGNAGE
RURAL/SHOULDER CONDITION**

FOR MINIMUM WIDTH OF CLEAR ZONE, REFER TO THE LATEST EDITION OF THE FDOT PLANS PREPARATION MANUAL

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES CURBSIDE LANDING PADS AND SIGNAGE	FIGURE 1-5
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

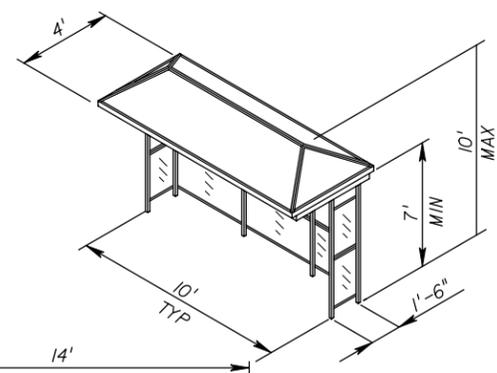
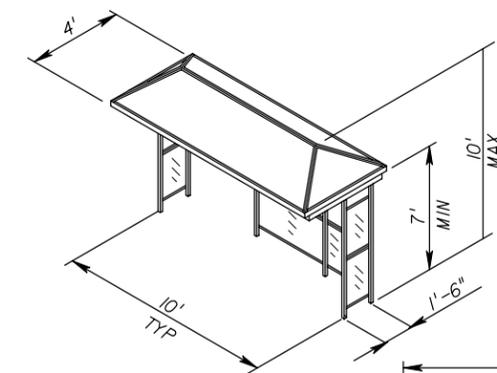


5'X14'X6" CONCRETE SLAB WITH TWO LAYERS OF 6"X6" W.W.M. ON VISQUEEN MEMBRANE AND ON WELL COMPACTED FILL (2500 PSF) TYP.

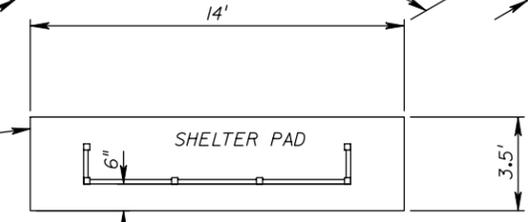
MEDIUM TYPICAL SHELTERS

8'X14'X6" CONCRETE SLAB WITH TWO LAYERS OF 6"X6" W.W.M. ON VISQUEEN MEMBRANE AND ON WELL COMPACTED FILL (2500 PSF) TYP.

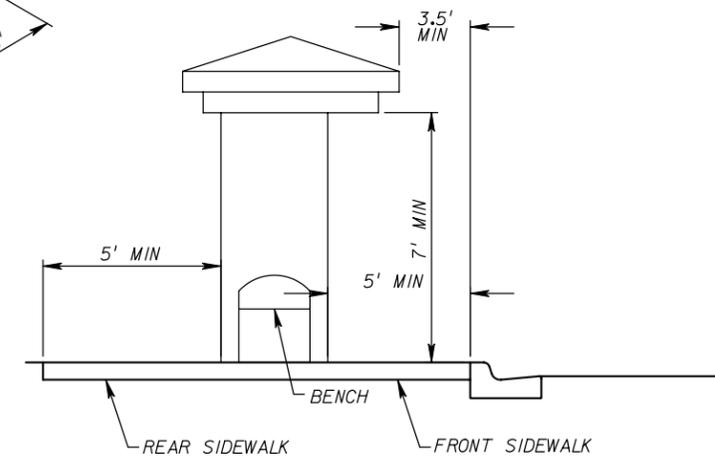
LARGE TYPICAL SHELTERS



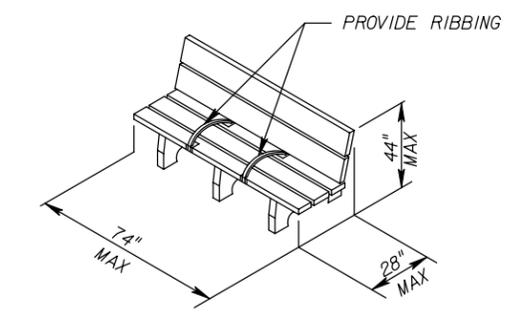
3.5'X14'X6" CONCRETE SLAB WITH TWO LAYERS OF 6"X6" W.W.M. ON VISQUEEN MEMBRANE AND ON WELL COMPACTED FILL (2500 PSF) TYP.



SMALL TYPICAL SHELTERS



MINIMUM CLEARANCES FOR SHELTERS URBAN CONDITION

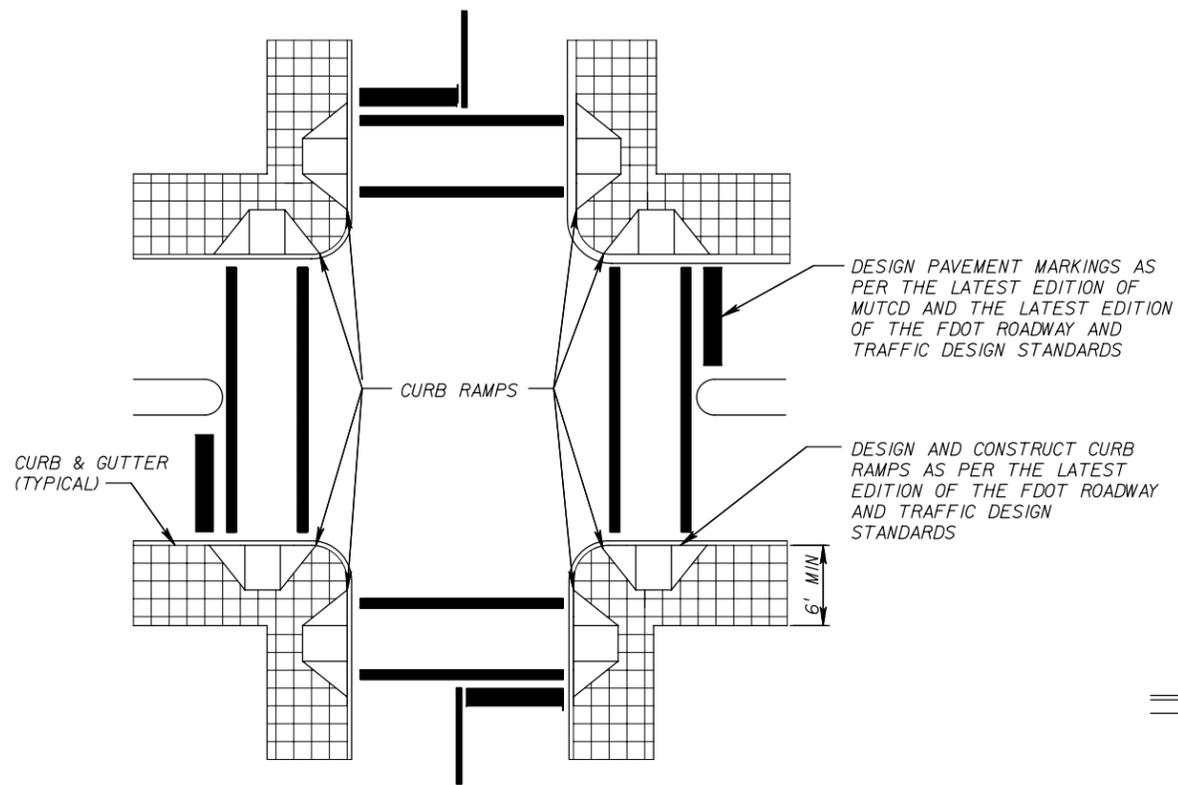


BENCH DIMENSIONS

BUS SHELTER NOTES:

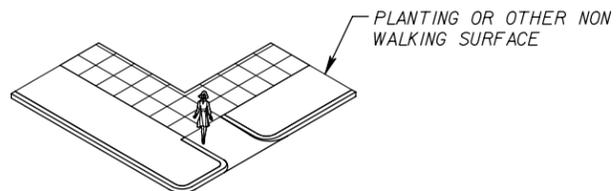
- COORDINATE WITH LOCAL TRANSIT AGENCY FOR BUS SHELTER DESIGN
- BUS SHELTERS ARE PROHIBITED IN MEDIANS.
- BUS SHELTERS SHALL BE LOCATED A MINIMUM OF 12 FT FROM THE INTERSECTION POINT OF CURVE/TANGENT.
- BUS SHELTERS SHALL NOT BE LOCATED WITHIN 15 FT OF A FIRE HYDRANT OR DISABLED PARKING SPACE.
- BICYCLE RACKS SHALL BE CONSIDERED AS PART OF THE SHELTER BASED ON COORDINATION WITH THE TRANSIT AGENCY.
- A CLEAR AREA OF 2 FT MINIMUM SHOULD BE PROVIDED BEHIND SHELTER FOR MAINTENANCE.
- BICYCLE RACKS SHALL BE LOCATED SO AS NOT TO BLOCK THE VIEW OF WAITING PASSENGERS.
- BUS SHELTERS SHALL BE LOCATED AT LEAST 15 FT (DESIRABLE) AND 7 FT (MINIMUM) FROM THE NEAREST UTILITY POLE.
- STRUCTURAL DESIGN TO BE PER ALL APPLICABLE LOCAL CODES.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES CURBSIDE BUS SHELTER DETAILS	FIGURE 1-6
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



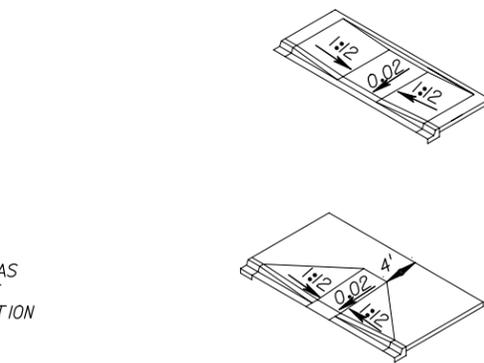
**TYPICAL URBAN INTERSECTION
(DESIRABLE RAMPS)**

NOTE: CURB RAMPS ARE TO BE COMPLETELY CONTAINED INSIDE THE CROSSWALKS.



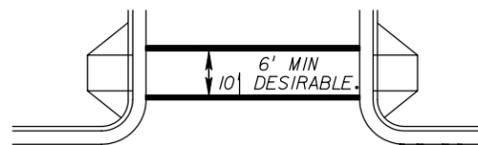
CURB RAMP / PLANTING STRIP COMBINATION

WHERE THE CURB RAMP IS COMPLETELY CONTAINED WITHIN A PLANTING STRIP OR OTHER NON-WALKING SURFACE, SO THAT PEDESTRIANS WOULD NOT NORMALLY CROSS THE SIDES, THE CURB RAMP SIDES CAN HAVE STEEP SIDES INCLUDING VERTICAL RETURNED CURBS.



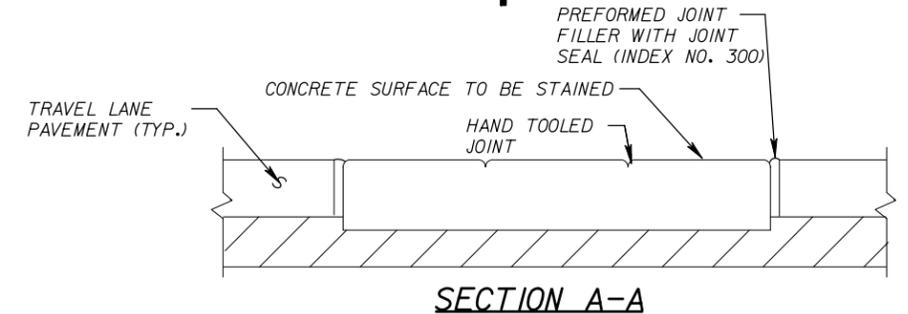
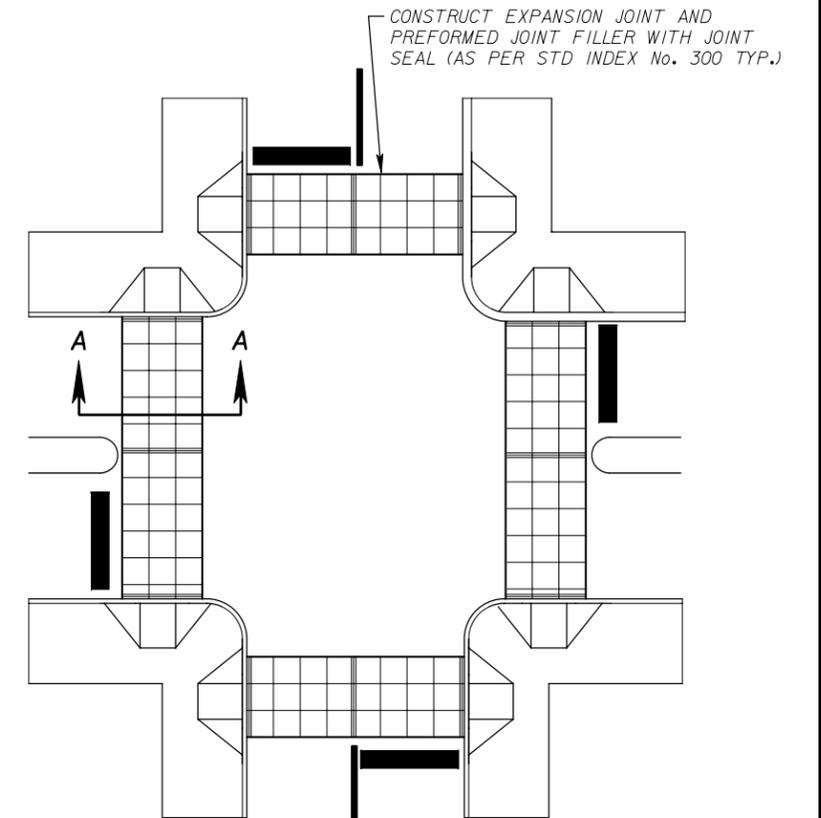
TYPICAL MID-BLOCK CURB RAMPS

FOR CURB RAMP DETAILS, REFER TO THE LATEST EDITION OF THE ROADWAY AND TRAFFIC DESIGN STANDARDS.



**ALTERNATIVE CROSSWALK
PAVEMENT MARKINGS**

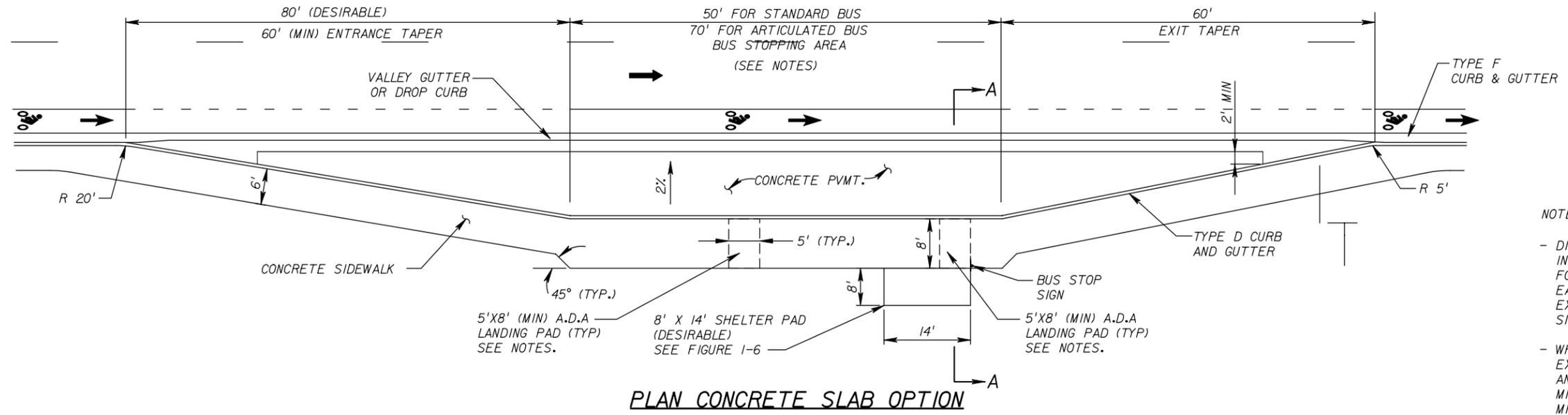
- NOTES:
- CURB RAMPS SHOULD BE INSTALLED EVERYWHERE THERE IS A CROSSWALK, MARKED OR UNMARKED.
 - CURB RAMPS MUST NOT INTERFERE WITH THE FREE ACCESS TO THE BUS STOP. CURB RAMPS MAY NOT BE INSTALLED AT BUS STOPPING LOCATIONS. INSTEAD, AN ADA LANDING PAD MUST BE INSTALLED FOLLOWING ALL RELEVANT GUIDELINES, SEE FIGURE 1-5.
 - SIDEWALKS SHOULD BE CONSTRUCTED ALONG BOTH SIDES OF ARTERIAL ROADWAYS THAT ARE NOT PROVIDED WITH SHOULDERS, EVEN THOUGH PEDESTRIAN TRAFFIC MAY BE LIGHT.



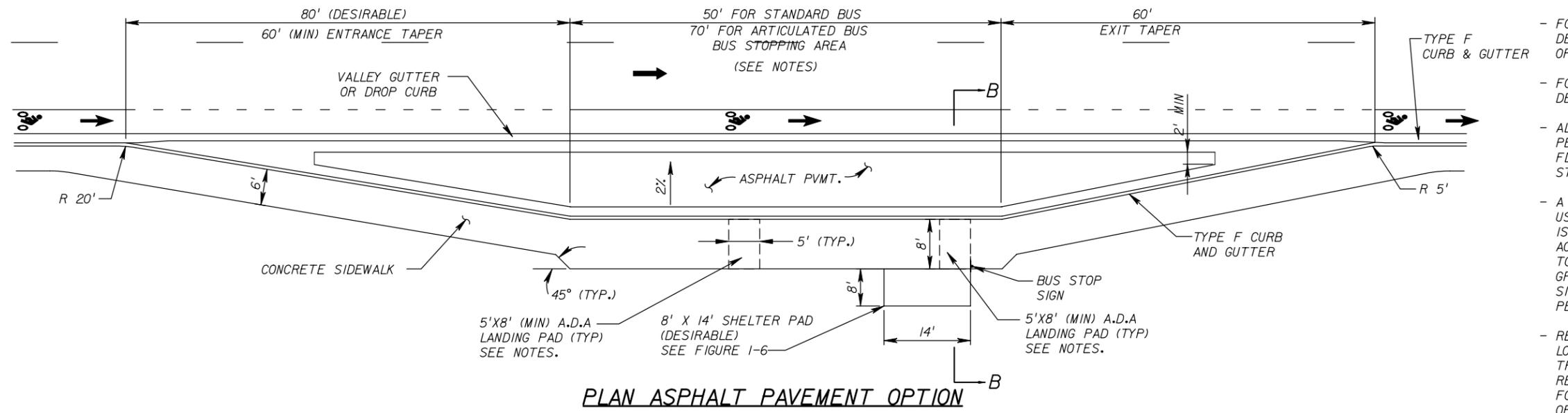
CONCRETE CROSSWALK TYPICAL SECTION

CONST. CONCRETE JOINTS PER INDEX 305

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES CURB RAMPS AND CONCRETE CROSSWALK	FIGURE 1-7
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

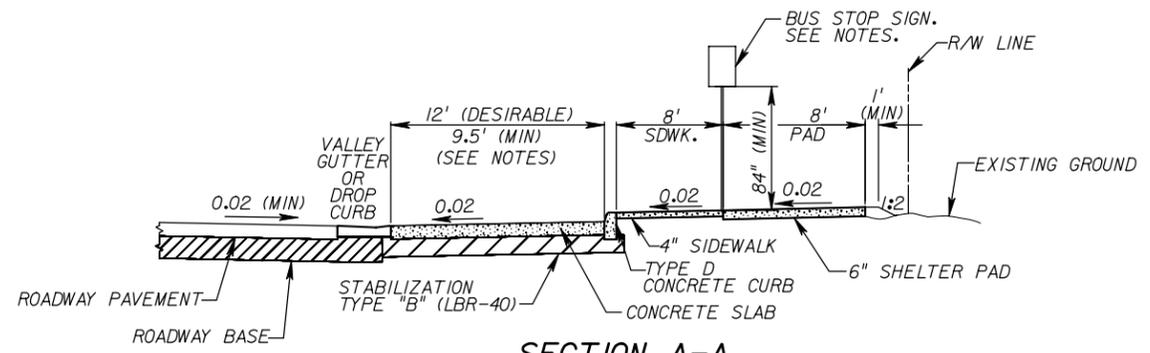


PLAN CONCRETE SLAB OPTION



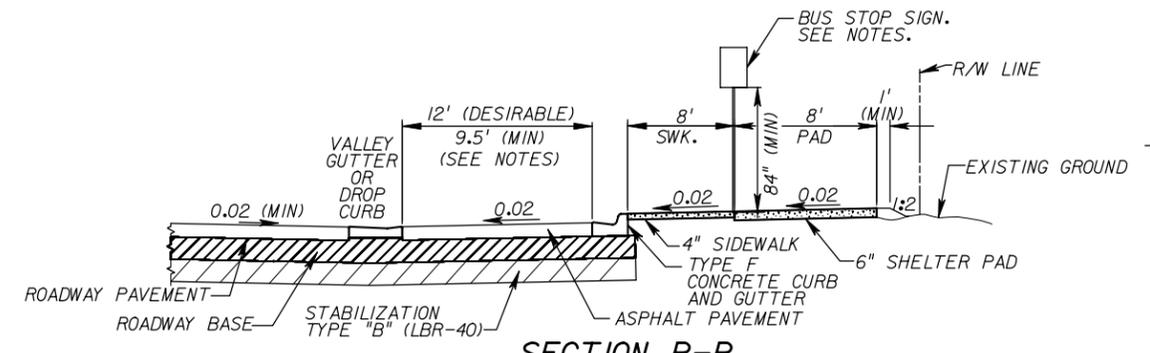
PLAN ASPHALT PAVEMENT OPTION

- NOTES:**
- DIMENSIONS SHOWN ARE FOR ONE BUS. INCREASE LENGTH OF BUS BAY BY 50' FOR EACH 40-FOOT BUS AND 70' FOR EACH 60-FOOT ARTICULATED BUS EXPECTED TO BE AT THE STOP SIMULTANEOUSLY.
 - WHEN NO BUS SHELTER IS USED, EXTEND THE SIDEWALK TO PROVIDE AN A.D.A. LANDING PAD WITH A MINIMUM CLEAR LENGTH OF 8' AND A MINIMUM CLEAR WIDTH OF 5'.
 - FOR CURB & GUTTER TRANSITION DETAILS, REFER TO LATEST VERSION OF FDOT STANDARD INDEXES.
 - FOR SHELTER AND SHELTER PAD DETAILS, REFER TO FIGURE I-6.
 - ALL CONCRETE JOINTS SHALL BE AS PER THE LATEST VERSION OF THE FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS.
 - A MID-BLOCK CROSSWALK CAN BE USED IN LOCATIONS WHERE THERE IS A MAJOR TRANSIT ORIENTED ACTIVITY CENTER OR THE DISTANCE TO THE NEXT INTERSECTION IS GREATER THAN 300 FEET. SIGNALIZATION MAY BE PROVIDED AS PER THE MUTCD.
 - REFER TO THE "GUIDELINES FOR THE LOCATION AND DESIGN OF BUS STOPS", TRANSIT COOPERATIVE RESEARCH PROGRAM REPORT 19, WASHINGTON D.C. 1996, FOR SPECIAL CONDITIONS AND DIMENSIONS OF BUS BAYS.
 - BUS STOP SIGN PANEL MUST BE LOCATED SUCH THAT A MINIMUM CLEARANCE OF 36" IS PROVIDED ON THE SIDEWALK.
 - DRAINAGE STRUCTURES ARE NOT TO BE LOCATED WITHIN THE BUS BAY.



SECTION A-A

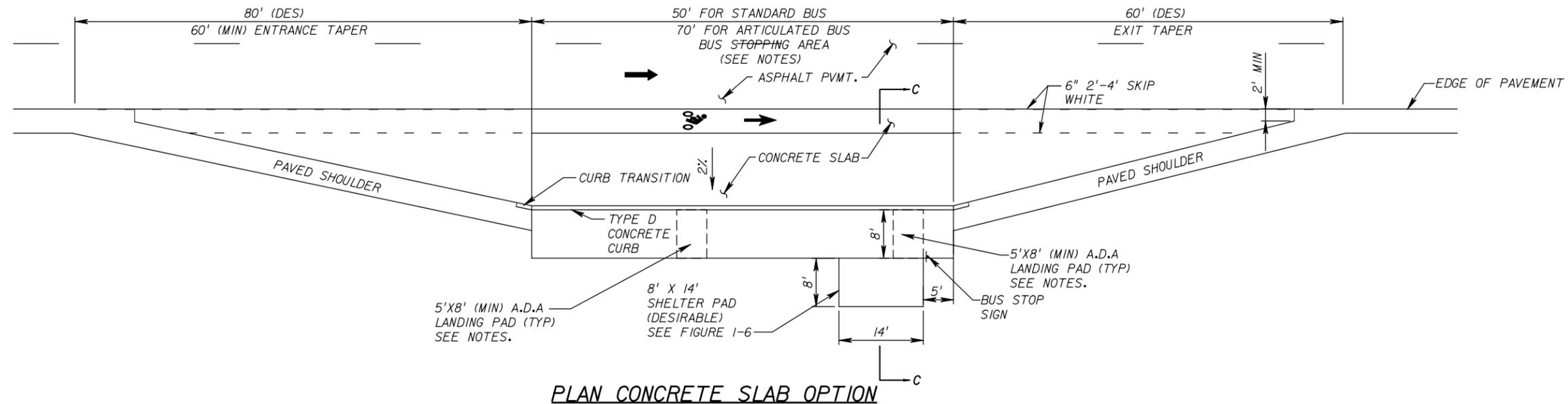
TYPICAL BUS BAY URBAN/CURB & GUTTER CONDITION WITH CONCRETE PAVEMENT
N.T.S.



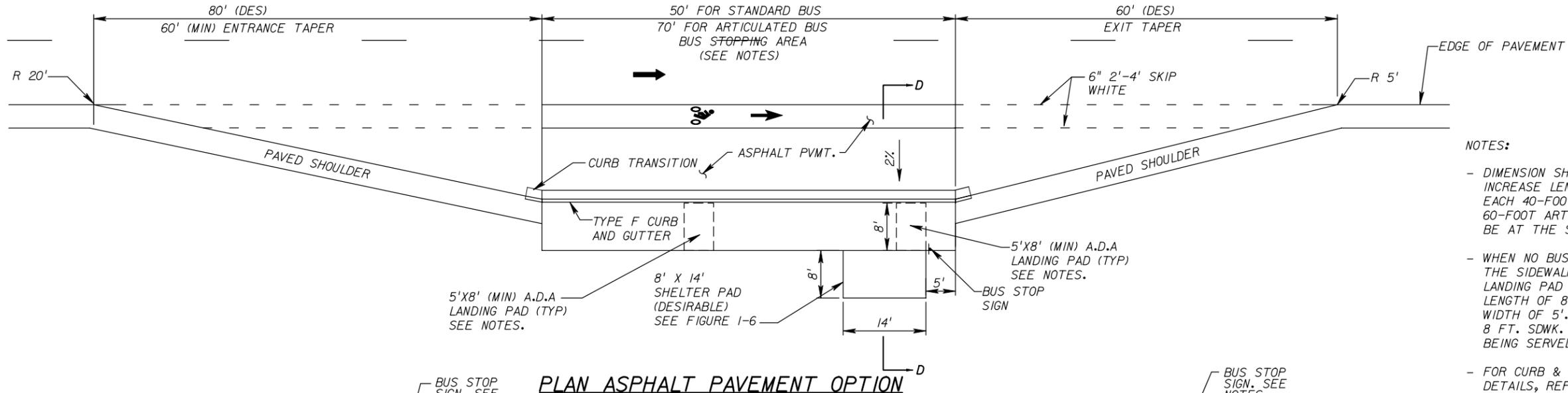
SECTION B-B

TYPICAL BUS BAY URBAN/CURB & GUTTER CONDITION WITH ASPHALT PAVEMENT
N.T.S.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES CLOSED BUS BAY LAYOUT URBAN/CURB & GUTTER	FIGURE I-8
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



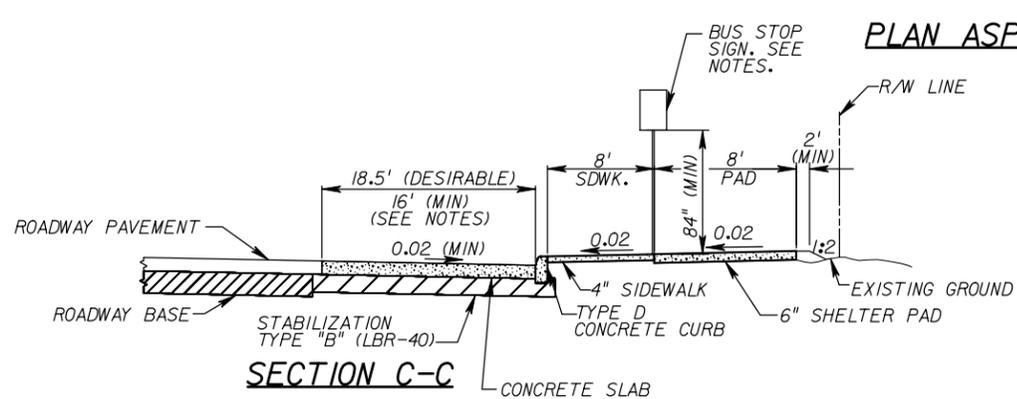
PLAN CONCRETE SLAB OPTION



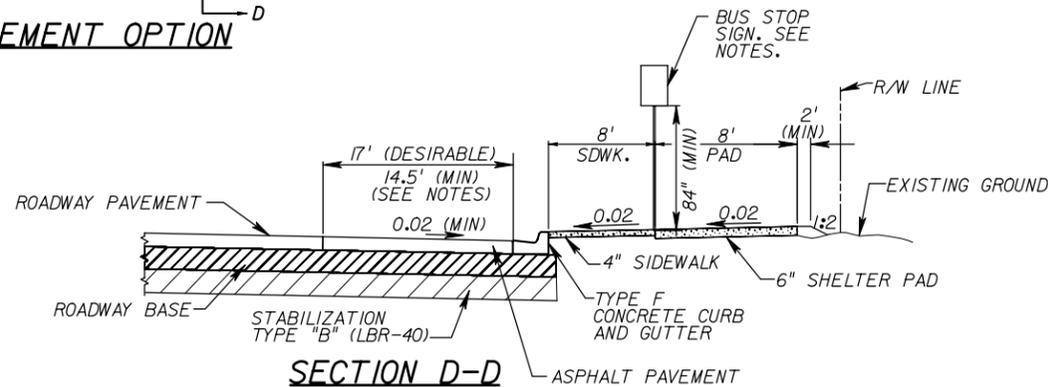
PLAN ASPHALT PAVEMENT OPTION

NOTES:

- DIMENSION SHOWN IS FOR ONE BUS. INCREASE LENGTH OF BUS BAY BY 50' FOR EACH 40-FOOT BUS AND 70' FOR EACH 60-FOOT ARTICULATED BUS EXPECTED TO BE AT THE STOP SIMULTANEOUSLY.
- WHEN NO BUS SHELTER IS USED, EXTEND THE SIDEWALK TO PROVIDE AN A.D.A. LANDING PAD WITH A MINIMUM CLEAR LENGTH OF 8' AND A MINIMUM CLEAR WIDTH OF 5'. IT IS DESIRABLE TO PROVIDE 8 FT. SDWK. CONNECTION TO LOCATION BEING SERVED.
- FOR CURB & GUTTER TRANSITION DETAILS, REFER TO LATEST VERSION OF FDOT STANDARD INDEX.
- FOR SHELTER AND SHELTER PAD DETAILS, REFER TO FIGURE I-6.
- ALL CONCRETE JOINTS SHALL BE AS PER THE LATEST VERSION OF FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS.
- BUS STOP SIGN PANEL MUST BE LOCATED SUCH THAT A MINIMUM CLEARANCE OF 36" IS PROVIDED ON THE SIDEWALK.
- DRAINAGE STRUCTURES SHOULD NOT BE LOCATED WITHIN THE BUS BAY.
- BUS BAY SIDEWALK SHOULD BE CONNECTED TO EXISTING SDWK. OR ACCESSIBLE TO SHOULDER.



TYPICAL BUS BAY RURAL/SHOULDER CONDITION WITH CONCRETE PAVEMENT
N.T.S.



TYPICAL BUS BAY RURAL/SHOULDER CONDITION WITH ASPHALT PAVEMENT
N.T.S.

REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID

**TRANSIT FACILITIES GUIDELINES
CLOSED BUS BAY LAYOUT
RURAL/SHOULDER**

FIGURE

I-9

URBAN RIGHT TURN LANE DESIGN LENGTH		
DESIGN SPEED (MPH)	* D ₁ (FT)	
	RIGHT TURN VALUE	THROUGH LANE VALUE
35	95'+QRT	20'+QT
40	105'+QRT	30'+QT
45	135'+QRT	35'+QT
50	190'+QRT	55'+QT

NOTE: THIS TABLE APPLIES TO ALL NEAR SIDE AND FAR SIDE BUS FACILITIES IN COMBINATION WITH RIGHT TURN LANES.

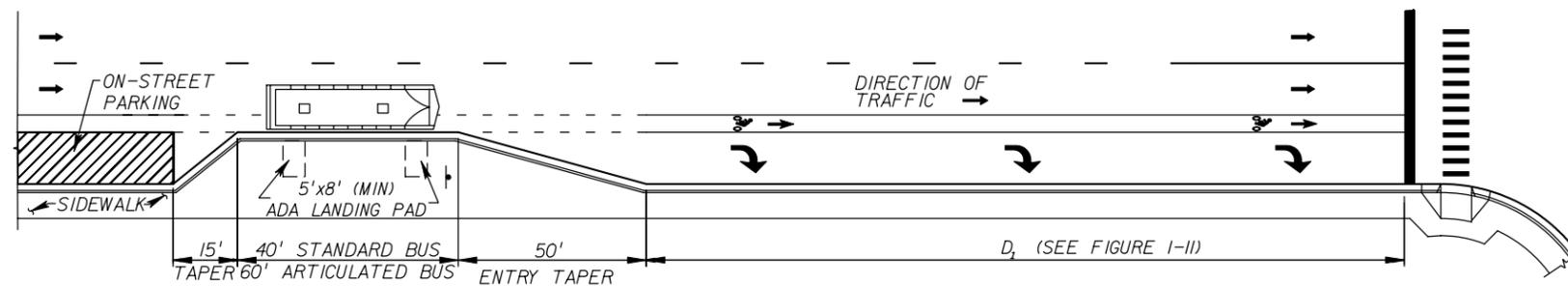
SOURCE: FDOT STANDARD INDEX 301 [D₁=(L₁+L₂)-50]

- * D₁ = UTILIZE LARGER OF TWO VALUES FOR NEAR SIDE STOPS AND RIGHT TURN VALUE FOR FAR SIDE STOPS.
- QRT= QUEUE STORAGE VALUE FOR THE RIGHT TURN MOVEMENT (FT).
- QT= QUEUE STORAGE VALUE FOR THE THROUGH MOVEMENT (FT).

NOTES FOR QUEUE STORAGE VALUES: (QRT OR QT)

- UTILIZE A 90% SUCCESS RATE VALUE FOR ALL NON-FIHS FACILITIES AND A 95% SUCCESS RATE FOR ALL FIHS FACILITIES. ALL QUEUE VALUES ARE TO BE OBTAINED FROM THE PEAK HOUR FOR THE DESIGN YEAR, AND SHALL BE BASED ON THE ADJUSTED MAXIMUM QUEUE REACH (AMQR).
- WHEN POSSIBLE AND DESIRABLE, PROVIDE MORE STORAGE WHERE THE PROJECTED VALUES APPEAR "LIGHT".
- UTILIZE A VALUE OF 25FT AVERAGE VEHICLE DISTANCE UP TO A VALUE OF 2% TRUCKS. FOR GREATER TRUCK PERCENTAGES, INCREASE THE AVERAGE VEHICLE DISTANCE BY 2 TO 3FT FOR EVERY 5% INCREASE IN TRUCK PERCENTAGE.
- A FORMAL QUEUE STUDY SHOULD BE PERFORMED AT EACH LOCATION. AT LOCATIONS WHERE A SPECIFIC QUEUE STUDY DOES NOT EXIST, UTILIZE A MINIMUM QUEUE STORAGE VALUE OF 100FT IN URBAN/SUBURBAN AREAS AND 50FT IN RURAL AREAS.
- THE QUEUE LENGTH IS TO BE MEASURED FROM THE RADIAL POINT OR, WHEN A STOP BAR IS REQUIRED, FROM THE STOP BAR.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES TURN LANE REQUIREMENTS	FIGURE
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		1-11



TYPICAL APPLICATION:

- URBAN AREA WHERE PARKING IS CRITICAL.
- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- ON STREET PARALLEL PARKING.
- RIGHT TURN LANE REQUIRED.

CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 40 MPH.

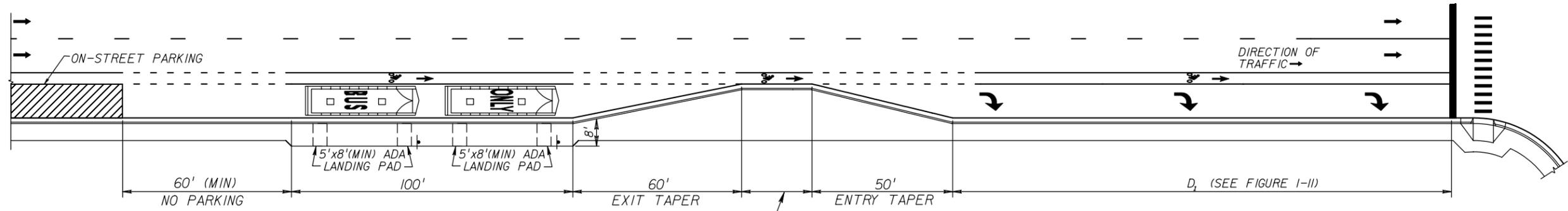
NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

N-1

NEAR SIDE NUB/BULB WITH ON STREET PARKING PRECEDING RIGHT TURN LANE

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES NEAR SIDE BUS STOPS	FIGURE
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		1-12



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- ON STREET PARALLEL PARKING.
- RIGHT TURN LANE REQUIRED.

25' DESIRABLE.
10' MIN
N-2A

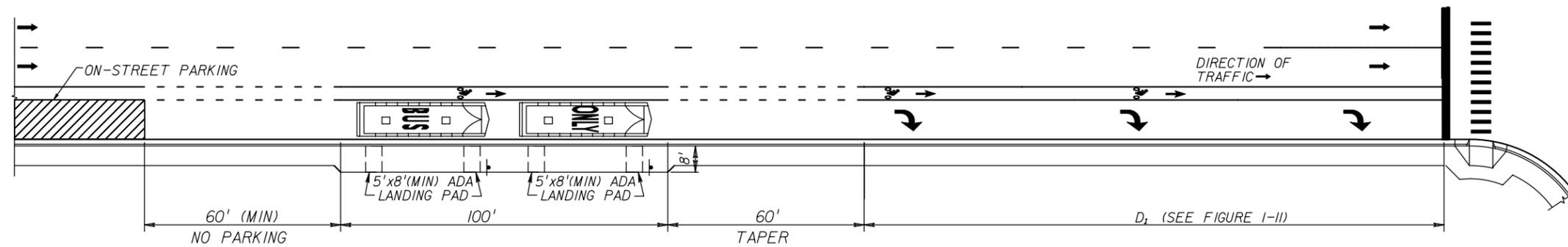
NEAR SIDE BUS BAY WITH ON STREET PARKING PRECEDING RIGHT TURN LANE

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤40 MPH.
- MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- ON STREET PARALLEL PARKING.
- RIGHT TURN LANE REQUIRED.

N-2B

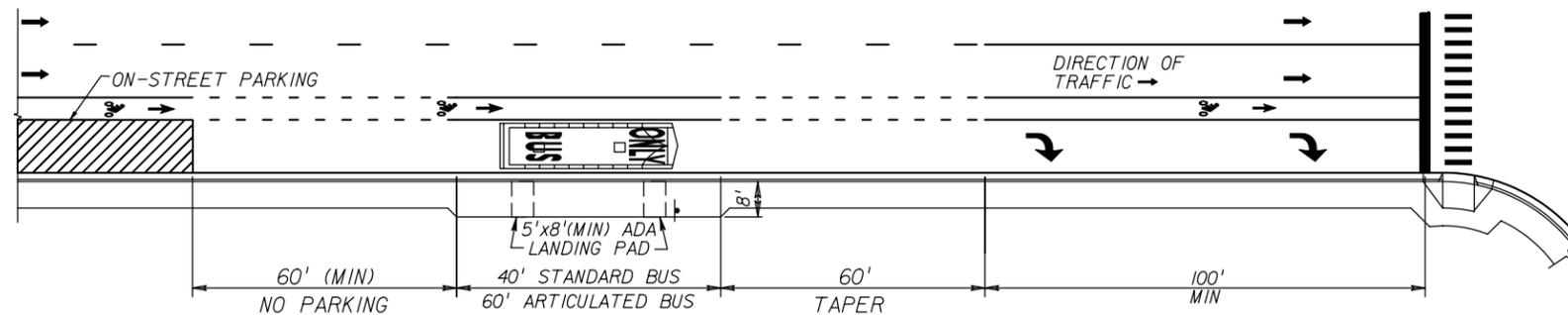
NEAR SIDE BUS BAY WITH ON STREET PARKING PRECEDING RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤40 MPH.
- SECOND MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- ON STREET PARALLEL PARKING.
- RIGHT TURN LANE REQUIRED.

N-2C

NEAR SIDE BUS BAY WITH ON STREET PARKING SHARED WITH RIGHT TURN LANE

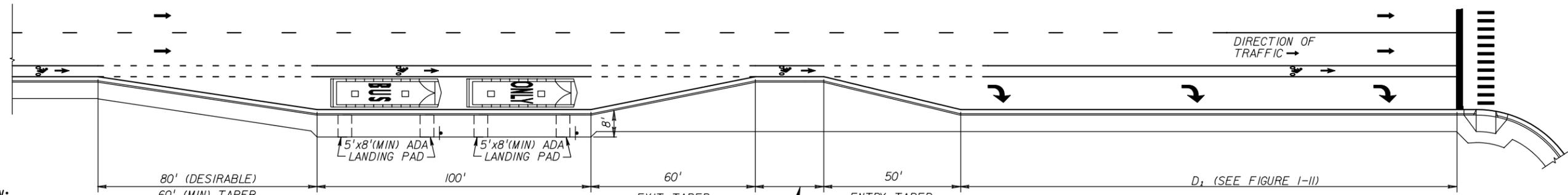
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤40 MPH.
- MINIMUM DESIGN FOR CONSTRAINED SITES.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES NEAR SIDE BUS BAYS	FIGURE 1-13
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- RIGHT TURN LANE REQUIRED.
- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.

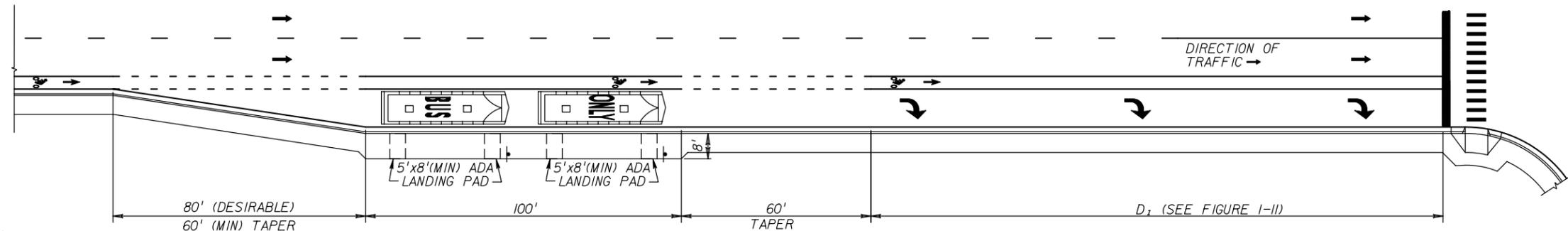
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- FOR BUS BAY DETAILS SEE FIGURE I-8 & I-9.
- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

N-3A
NEAR SIDE BUS BAY
PRECEDING RIGHT TURN LANE



TYPICAL APPLICATION:

- RIGHT TURN LANE REQUIRED.
- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.

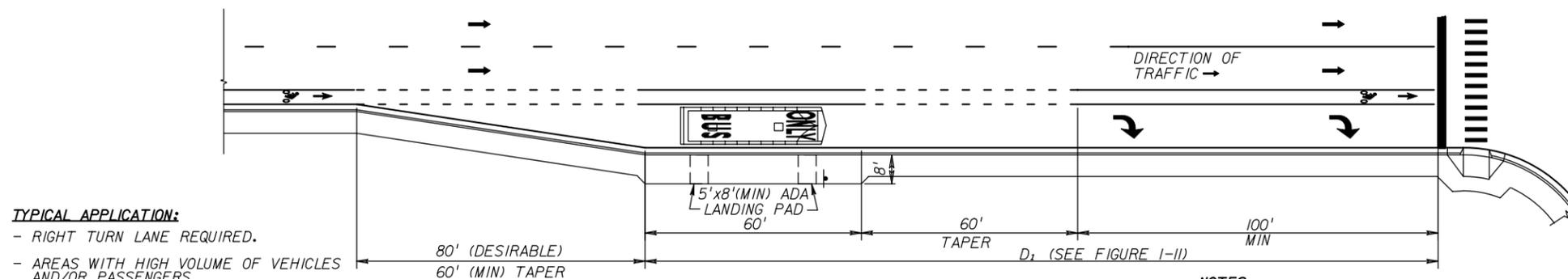
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- SECOND MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

N-3B
NEAR SIDE BUS BAY PRECEDING
RIGHT TURN LANE WITH SHARED TAPER



TYPICAL APPLICATION:

- RIGHT TURN LANE REQUIRED.
- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.

CONDITIONS:

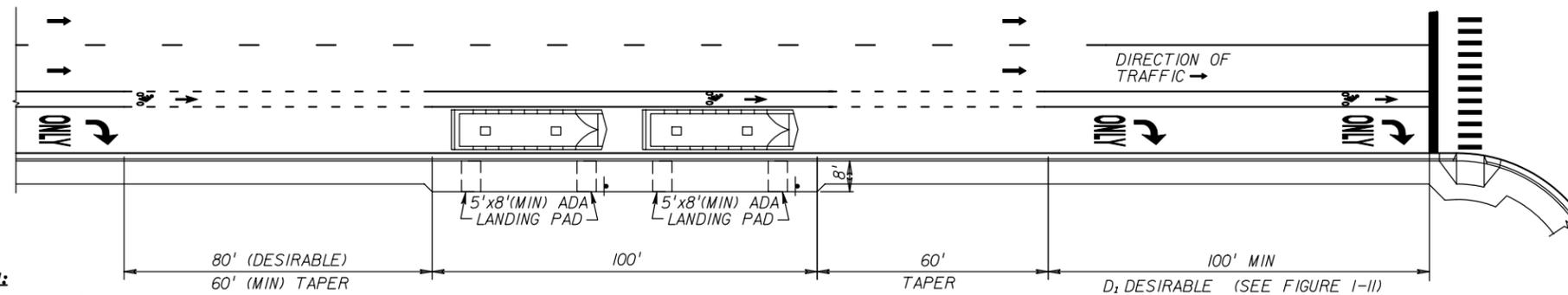
- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- MINIMUM DESIGN FOR CONSTRAINED SITES.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

N-3C
NEAR SIDE BUS BAY
SHARED WITH RIGHT TURN LANE

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES NEAR SIDE BUS BAYS	FIGURE I-14
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

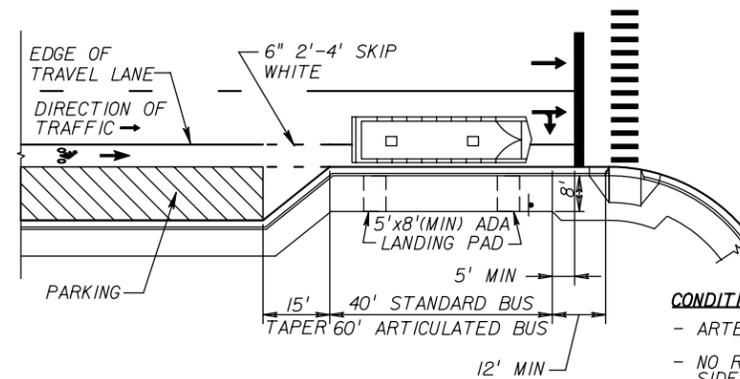


TYPICAL APPLICATION:
- RIGHT TURN LANE DROP LANE.

CONDITIONS:
- ARTERIAL DESIGN SPEED ≤ 40 MPH.

N-4
NEAR SIDE BUS BAY WITH PRECEDING LANE DROP RIGHT TURN LANE

NOTES:
- BUS STOP AREA SHOWN FOR TWO BUSES.
- SUCCEEDING QUEUE BYPASS TO BE CONSIDERED.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

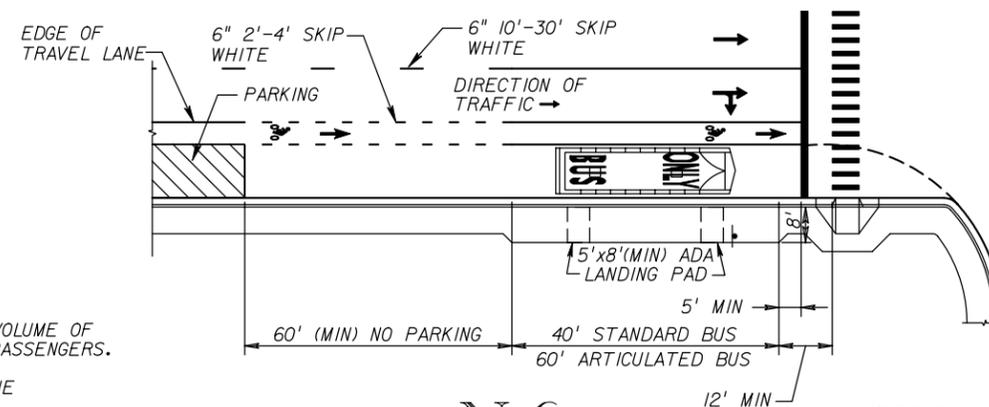


TYPICAL APPLICATION:
- URBAN AREA WHERE PARKING IS CRITICAL.
- AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
- ON STREET PARALLEL PARKING.
- NO RIGHT TURN LANE

N-5
NEAR SIDE NUB/BULB WITH ON-STREET PARKING

CONDITIONS:
- ARTERIAL DESIGN SPEED ≤ 40 MPH.
- NO RIGHT TURN BAY ON NEAR SIDE OF INTERSECTION.

NOTES:
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



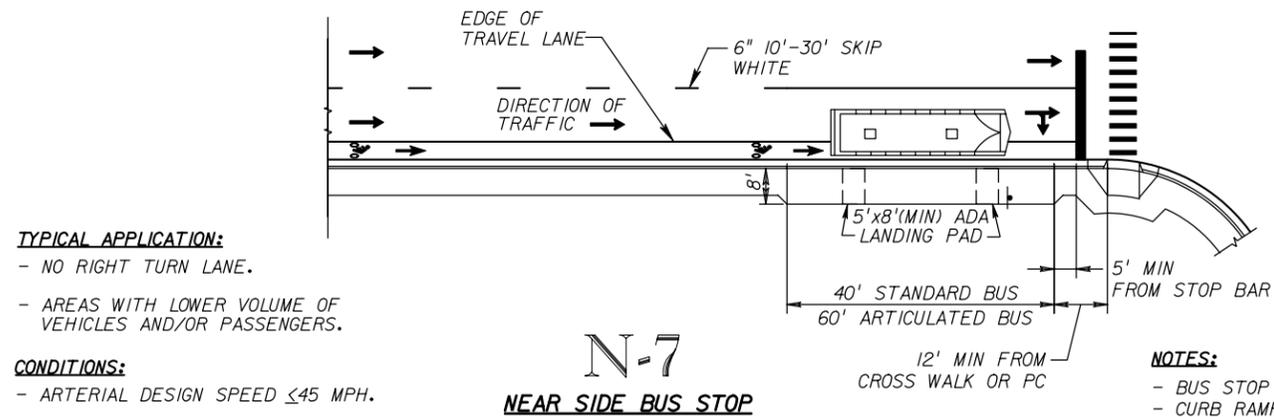
TYPICAL APPLICATION:
- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- NO RIGHT TURN LANE
- ON STREET PARALLEL PARKING.

N-6
NEAR SIDE BUS BAY WITH ON-STREET PARKING

CONDITIONS:
- ARTERIAL DESIGN SPEED ≤ 40 MPH.

NOTES:
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES NEAR SIDE BUS BAYS/STOPS	FIGURE 1-15
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

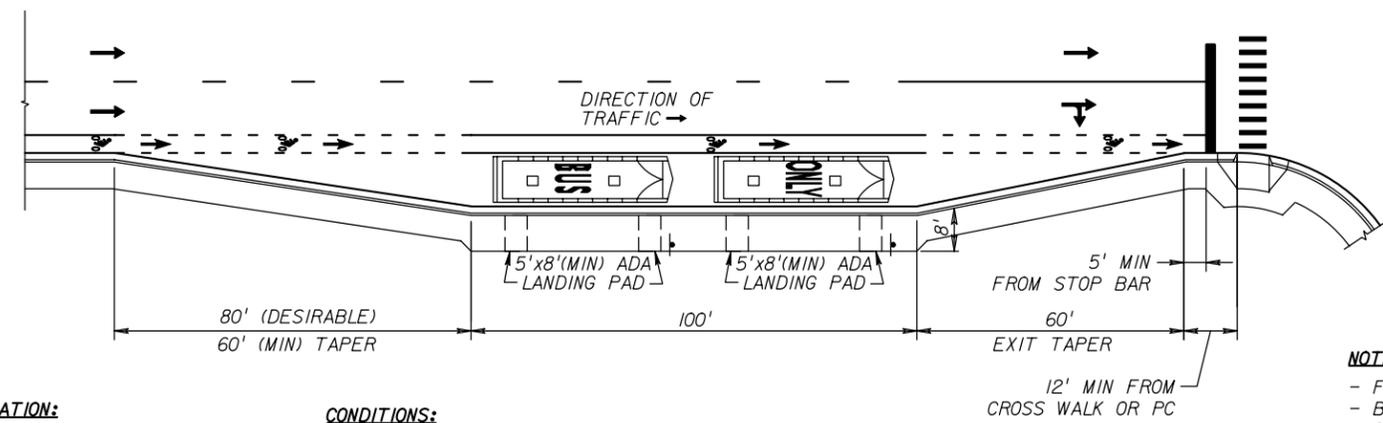


TYPICAL APPLICATION:
 - NO RIGHT TURN LANE.
 - AREAS WITH LOWER VOLUME OF VEHICLES AND/OR PASSENGERS.

CONDITIONS:
 - ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:
 - BUS STOP AREA SHOWN FOR ONE BUS.
 - CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

N-7
NEAR SIDE BUS STOP



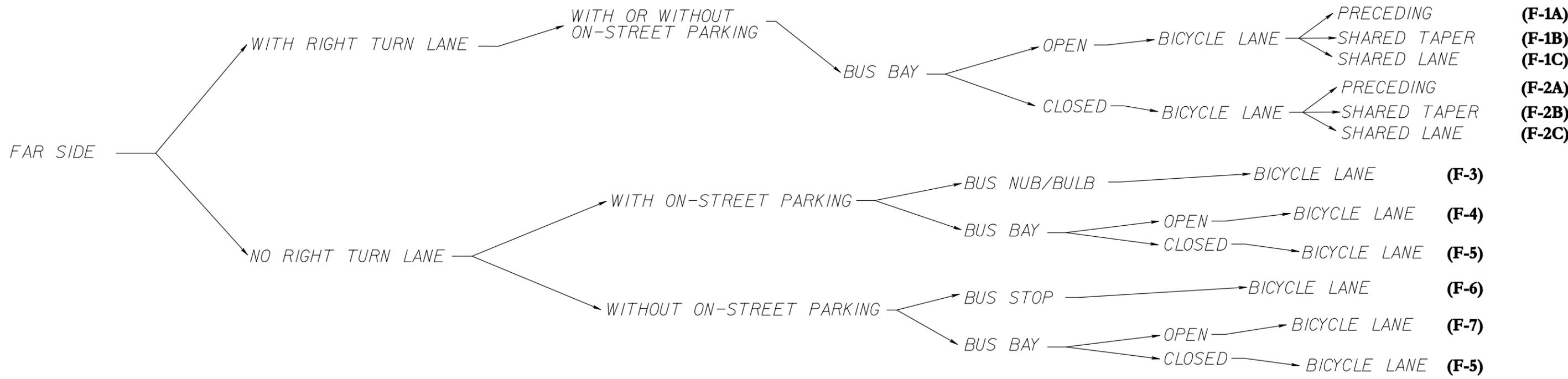
TYPICAL APPLICATION:
 - NO RIGHT TURN LANE.
 - AREAS WITH HIGHER VOLUME OF VEHICLES AND/OR PASSENGERS.

CONDITIONS:
 - ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:
 - FOR BUS BAY DETAILS SEE FIGURE 1-8 & 1-9.
 - BUS STOP AREA SHOWN FOR TWO BUSES.
 - CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

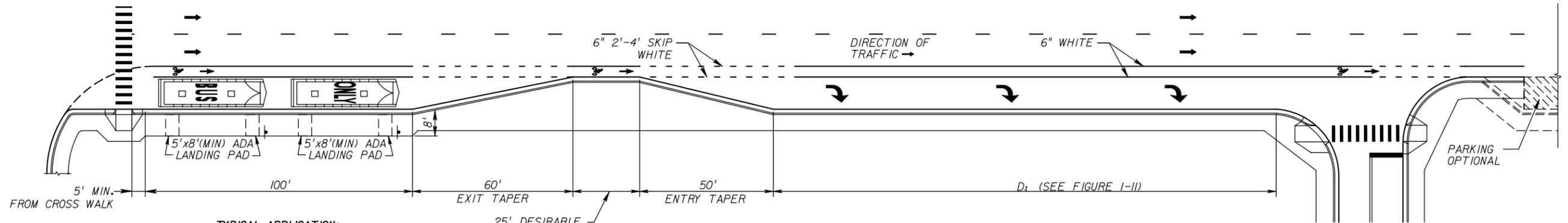
N-8
NEAR SIDE BUS BAY

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES NEAR SIDE BUS BAYS/STOPS	FIGURE 1-16
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



LEGEND
(N-1) – DETAIL DRAWING

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITY GUIDELINES FAR SIDE BUS FACILITY DECISION TREE	FIGURE 1-17
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION			



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1A

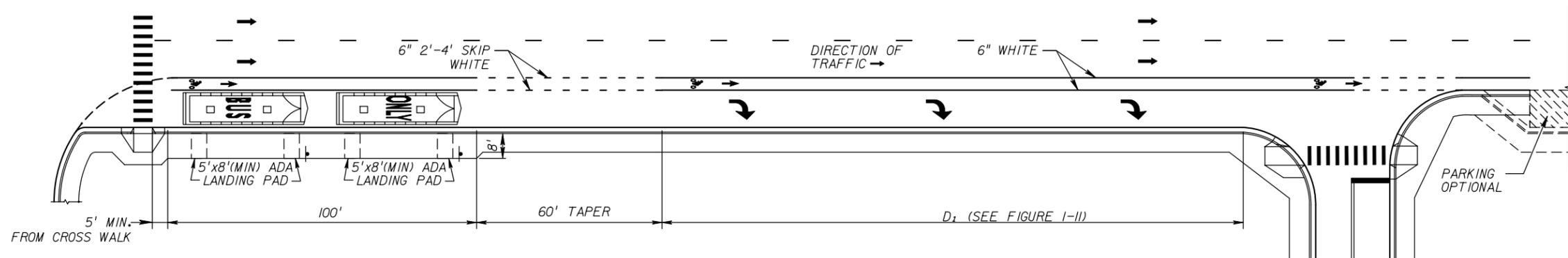
FAR SIDE OPEN BUS BAY PRECEDING RIGHT TURN LANE

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
- MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1B

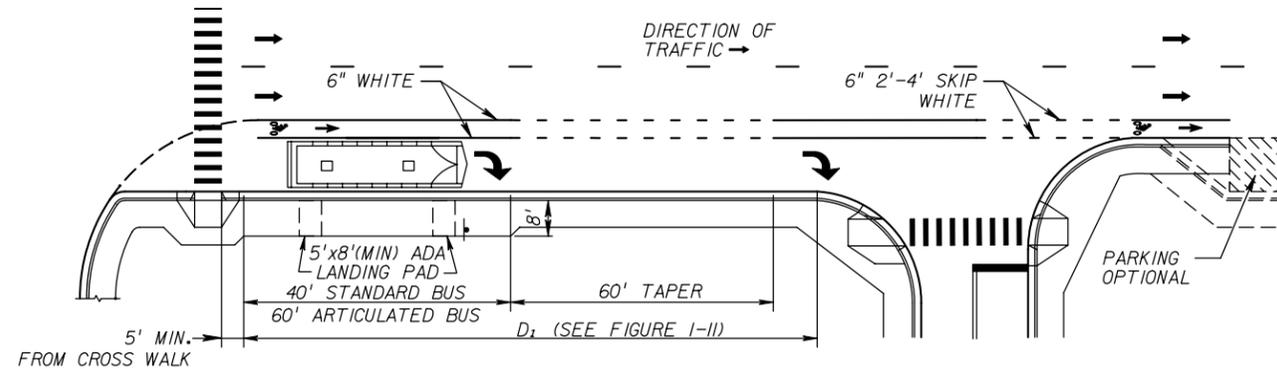
FAR SIDE OPEN BUS BAY PRECEDING RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
- SECOND MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT NEAR SIDE AND FAR SIDE OF INTERSECTION.

F-1C

FAR SIDE OPEN BUS BAY SHARED WITH RIGHT TURN LANE

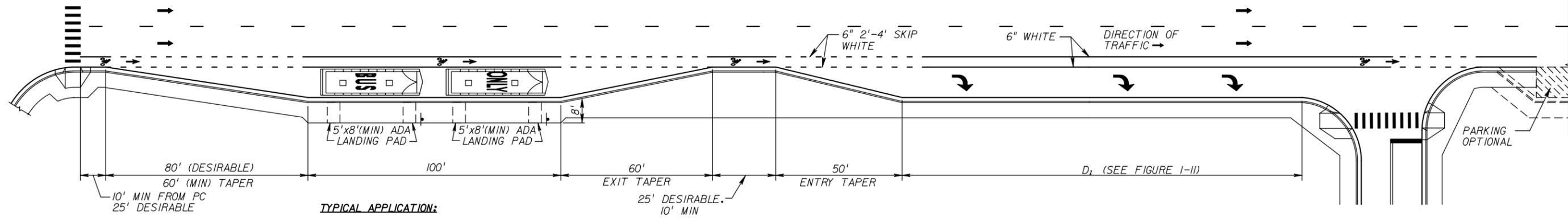
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS ROADWAY.
- MINIMUM DESIGN FOR CONSTRAINED SITES.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES FAR SIDE BUS BAYS	FIGURE 1-18
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

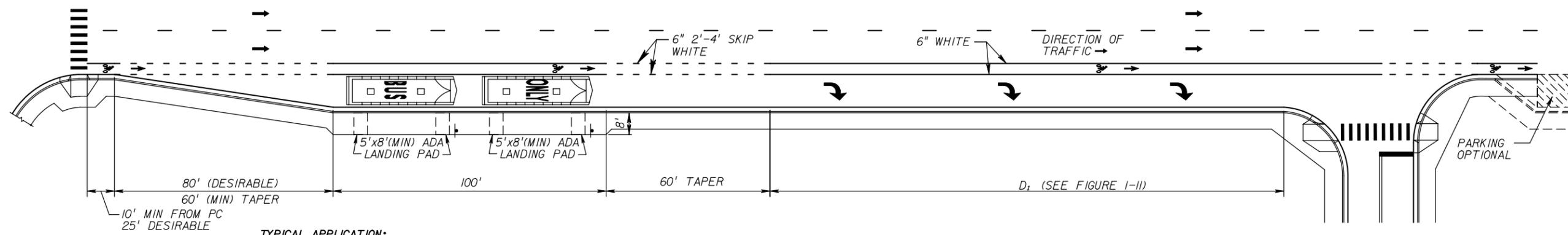
F-2A
FAR SIDE CLOSED BUS BAY PRECEDING RIGHT TURN LANE

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- FOR BUS BAY DETAILS SEE FIGURE 1-8 & 1-9.
- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

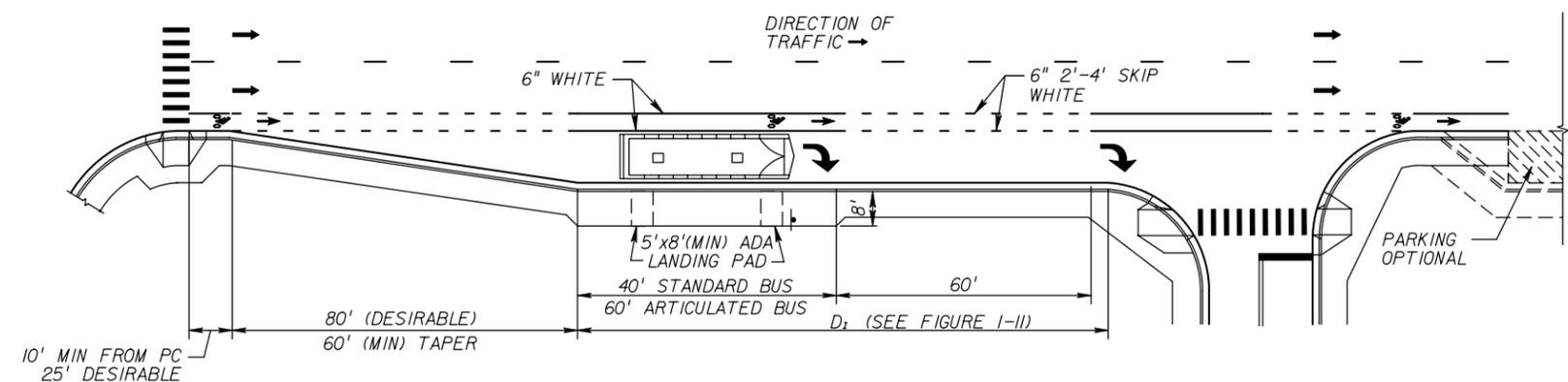
F-2B
FAR SIDE CLOSED BUS BAY PRECEDING RIGHT TURN LANE WITH SHARED TAPER

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- SECOND MOST DESIRABLE DESIGN IF VIABLE.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT FAR SIDE OF INTERSECTION.

F-2C
FAR SIDE CLOSED BUS BAY SHARED WITH RIGHT TURN LANE

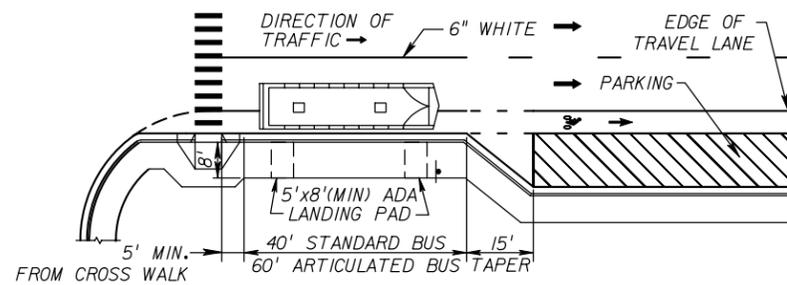
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- MINIMUM DESIGN FOR CONSTRAINED SITES.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES FAR SIDE BUS BAYS	FIGURE 1-19
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- URBAN AREA WHERE PARKING IS CRITICAL.
- AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK.
- ON-STREET PARALLEL PARKING.

F-3

**FAR SIDE NUB/BULB
WITH ON-STREET PARKING**

CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 40 MPH.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

REVISIONS

DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

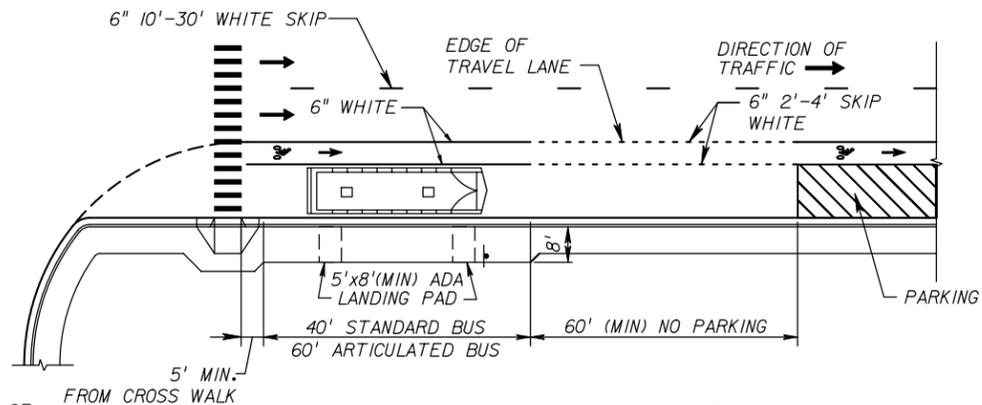
STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID

**TRANSIT FACILITY GUIDELINES
FAR SIDE BUS STOPS**

FIGURE

1-20



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.
- RIGHT TURN BAY AT NEAR SIDE OF INTERSECTION, AND NO FAR SIDE RIGHT TURN LANE.
- ON STREET PARALLEL PARKING.

CONDITIONS:

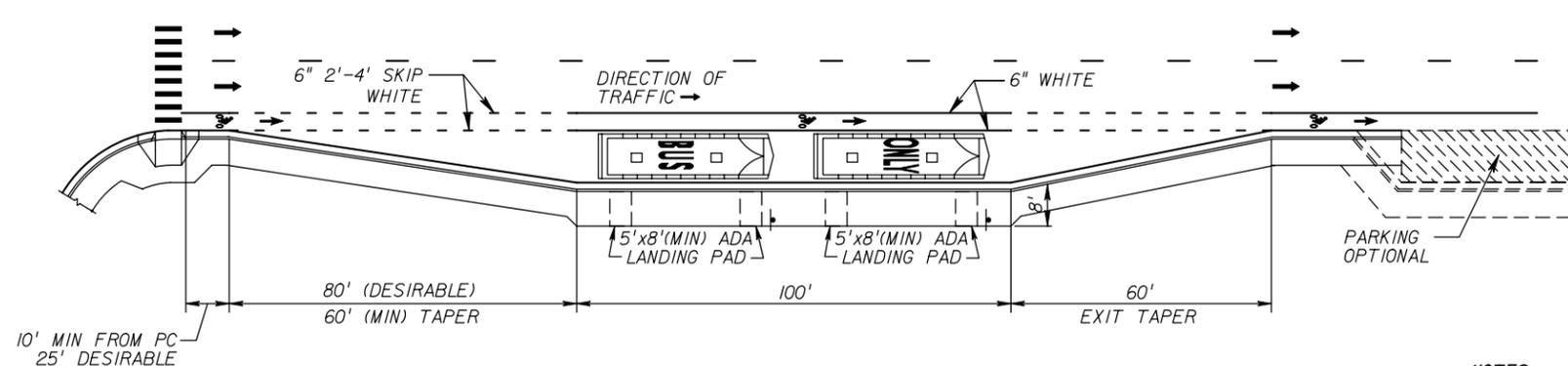
- ARTERIAL DESIGN SPEED ≤ 40 MPH.
- OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS DIVIDED ROADWAYS.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

F-4

FAR SIDE OPEN BUS BAY WITH ON STREET PARKING



TYPICAL APPLICATION:

- NO FAR SIDE RIGHT TURN LANE.
- AREAS WITH HIGHER VOLUME OF VEHICLES AND/OR PASSENGERS.

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.

NOTES:

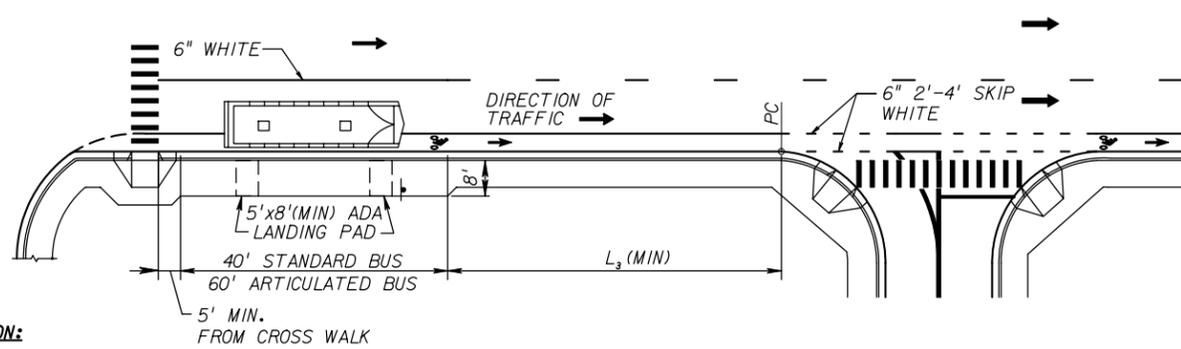
- FOR BUS BAY DETAILS SEE FIGURE 1-8 & 1-9.
- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

F-5

FAR SIDE CLOSED BUS BAY

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES FAR SIDE BUS BAYS	FIGURE 1-21
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

FAR SIDE BUS STOP PLACEMENT	
DESIGN SPEED (MPH)	L ₃
35	75'
40	75'
45	100'
50	135'



TYPICAL APPLICATION:

- NO FAR SIDE RIGHT TURN LANE.
- AREAS WITH LOWER VOLUME OF VEHICLES AND/OR PASSENGERS.

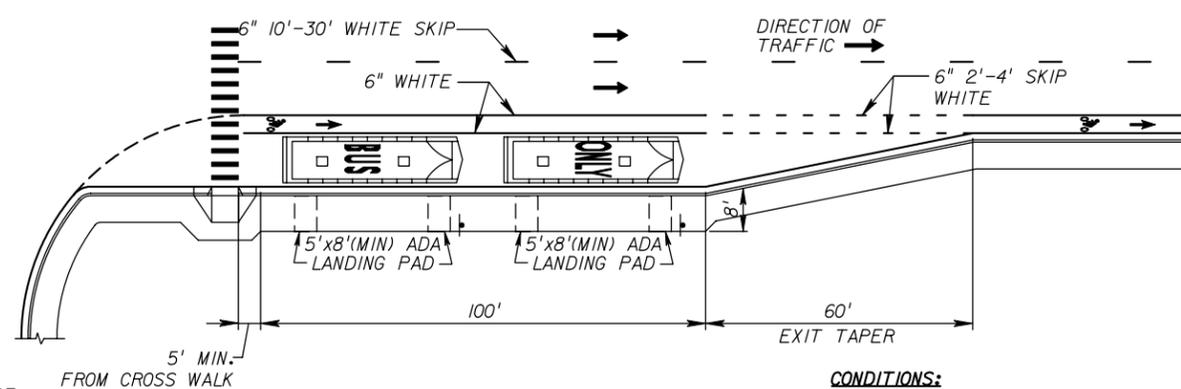
F-6
FAR SIDE BUS STOP WITHOUT RIGHT TURN LANE

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.



TYPICAL APPLICATION:

- RIGHT TURN BAY AT NEAR SIDE OF INTERSECTION AND NO RIGHT TURN LANE AT FAR SIDE OF INTERSECTION.
- AREAS WITH HIGHER VOLUME OF VEHICLES AND/OR PASSENGERS.

F-7
FAR SIDE OPEN BUS BAY

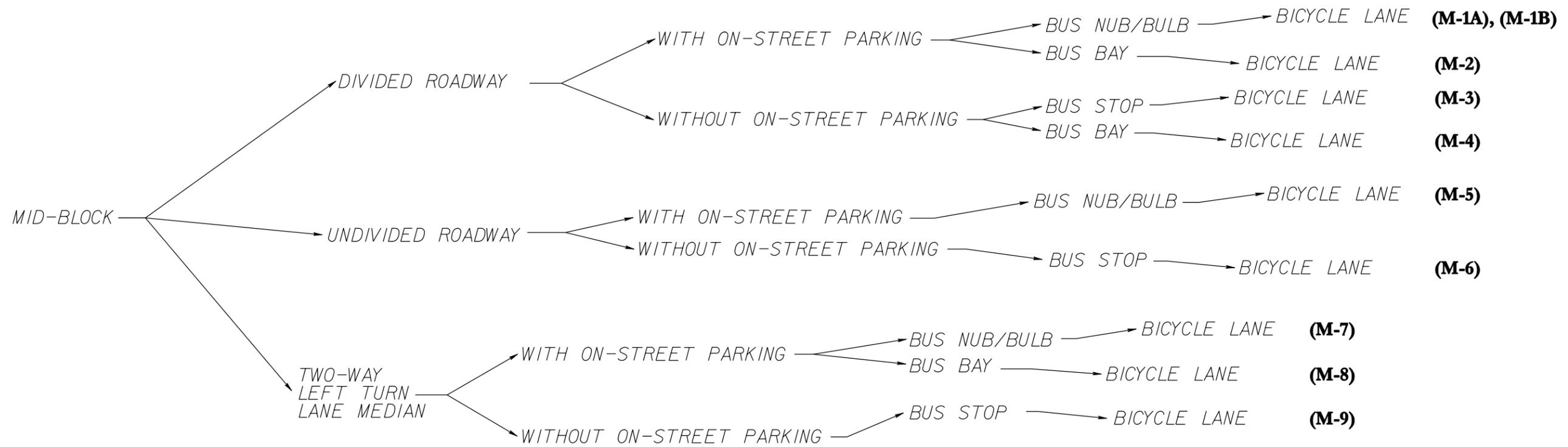
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.
- OPEN BUS BAY DESIGN RECOMMENDED FOR 4 LANE OR LESS DIVIDED ROADWAYS.

NOTES:

- BUS STOP AREA SHOWN FOR TWO BUSES.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

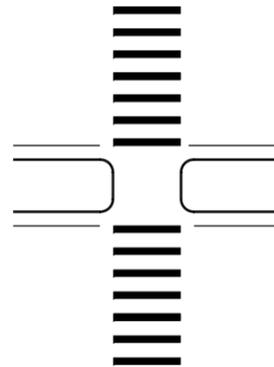
REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES FAR SIDE BUS BAYS/STOPS	FIGURE 1-22
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



LEGEND

(M-1) – DETAIL DRAWING

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITY GUIDELINES MID-BLOCK BUS FACILITY DECISION TREE	FIGURE 1-23
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION			



MEDIAN OPTION 1
(STRAIGHT MEDIAN WITH
STRAIGHT CROSSWALK)

NOTE: FOR 3 OR MORE LANE ROADWAY SECTIONS MIDBLOCK CROSSINGS MUST BE SIGNALIZED AND APPROVED BY FDOT'S TRAFFIC OPERATIONS DEPARTMENT.

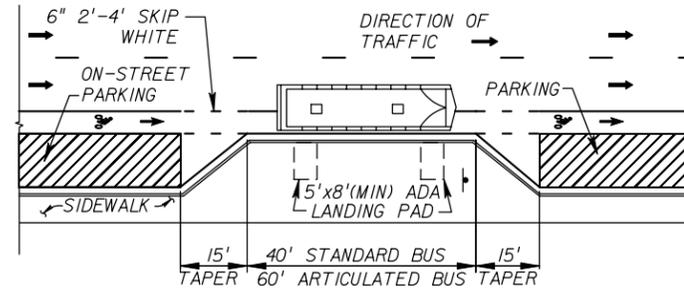
CROSSWALK CLEAR ZONE REQUIREMENTS							
DESIGN SPEED (MPH)	**S.S.D. (FT)	DIMENSION DL (FT)					
		MEDIAN/SIDEWALK WIDTH					
		2'	4'	6'	8'	10'	≥ 12'
30	200	100	135	150	160	165	170
35	250	125	165	190	200	210	215
40	300	150	200	225	240	250	260
45	350	175	235	265	280	290	300

** S.S.D. - STOPPING SIGHT DISTANCE

MID-BLOCK CROSSING NOTES:

- USE THE SAME PARAMETERS FOR ROADWAYS WITH MORE THAN FOUR LANES.
- INSTALL ADVANCE WARNING SIGNS AS PER THE MUTCD AND FDOT STANDARDS TO WARN MOTORISTS OF ONCOMING CROSSWALK.
- CLEAR AREA SHOULD BE FREE OF ALL FIXED OBJECTS SUCH AS LIGHT/UTILITY POLES, SIGNAL EQUIPMENT, TREES, VEGETATION, STREET FURNITURE, ETC. THAT WOULD OBSTRUCT THE VIEW OF PEDESTRIANS. CROSSING WARNING SIGNS ARE EXCLUDED FROM THE CLEAR AREA REQUIREMENTS.
- REFER TO FDOT INDEX 17346 OF THE LATEST EDITION OF THE FDOT ROADWAY AND TRAFFIC DESIGN STANDARDS FOR TRAFFIC SIGN AND CROSSING SIGNAL LOCATION.
- PARKING IS PROHIBITED FOR 100' IN ADVANCE OF THE CROSSWALK.
- PROVIDE AN EVEN WALKING SURFACE ACCROSS THE THE MEDIAN BY DEPRESSING THE CURBING AT THE CROSSING LOCATION. CURB RAMPS SHOULD NOT BE USED.

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS DETAILS	FIGURE
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		1-24



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
- MID-BLOCK BUS STOP NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- ON-STREET PARALLEL PARKING.

CONDITIONS:

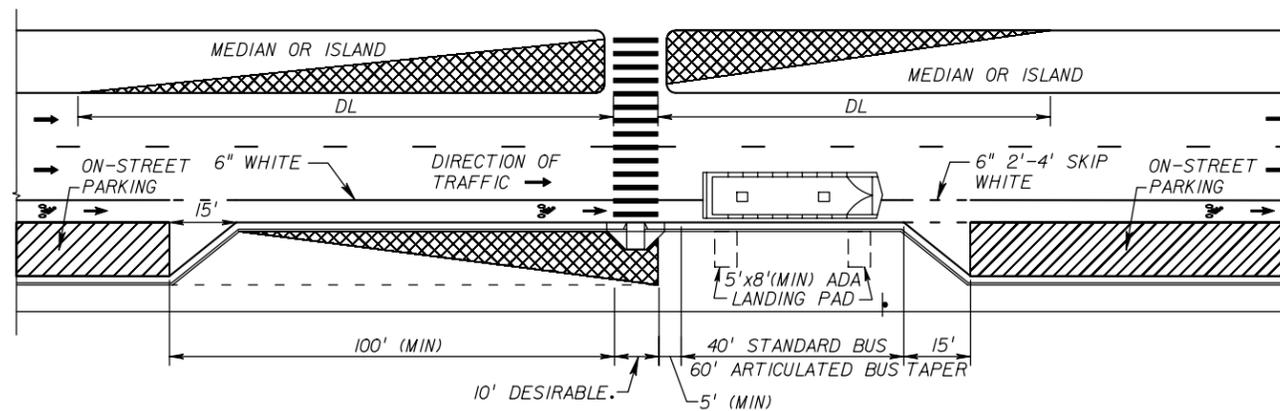
- ARTERIAL DESIGN SPEED ≤40 MPH.
- ON STREET PARKING PROVIDED.

NOTES:

- PEDESTRIAN RAMP AND CROSSWALK (WITH OPTIONAL PEDESTRIAN SIGNAL) CAN BE PROVIDED IF NO CONFLICT IS CREATED WITH A.D.A. LANDING PADS.
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.

M-1A

**MID-BLOCK NUB/BULB WITH ON-STREET PARKING
(NO MID-BLOCK CROSSWALK)**



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
- MID-BLOCK BUS STOP NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- ON-STREET PARALLEL PARKING.

CONDITIONS:

- ARTERIAL DESIGN SPEED ≤40 MPH.
- ON STREET PARKING PROVIDED.

NOTES:

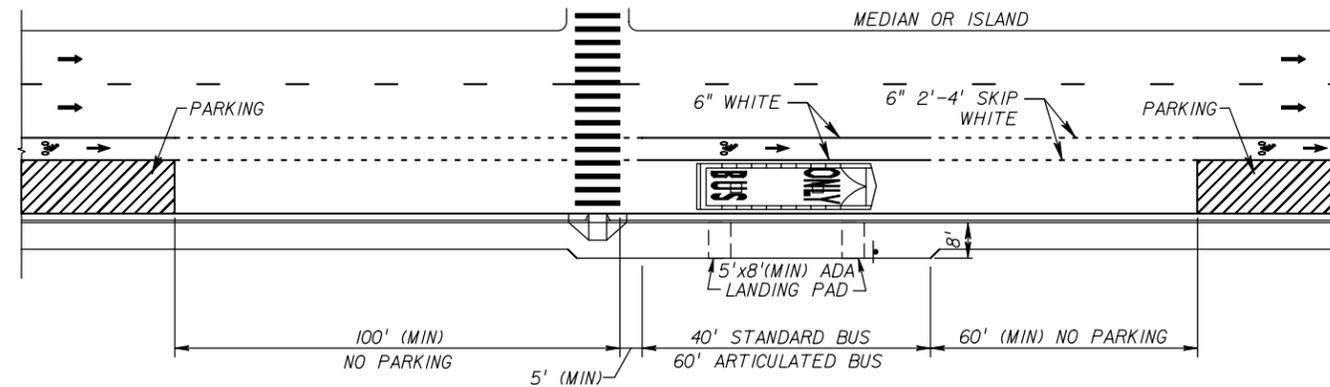
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE I-24.

M-1B

**MID-BLOCK NUB/BULB ON A
DIVIDED ROADWAY WITH ON-STREET PARKING**

- PARKING AREA
- CLEAR AREA (SEE FIGURE I-24 FOR DETAILS)

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS STOPS	FIGURE I-25
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND STREET PARKING EXISTS. WHEN PEDESTRIAN VOLUME IS HIGH, NUB ALTERNATIVE IS PREFERABLE.

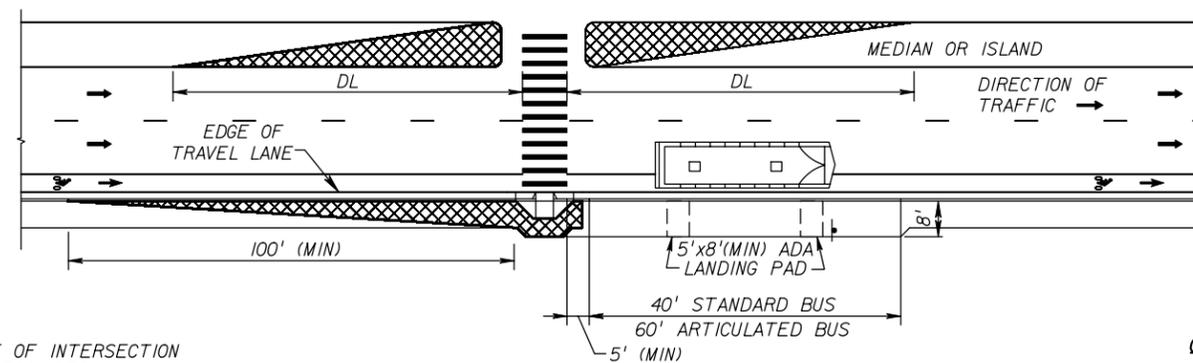
CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

M-2
MID-BLOCK BUS BAY ON A DIVIDED ROADWAY WITH ON-STREET PARKING (NO NUB ALTERNATIVE)



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 45 MPH.

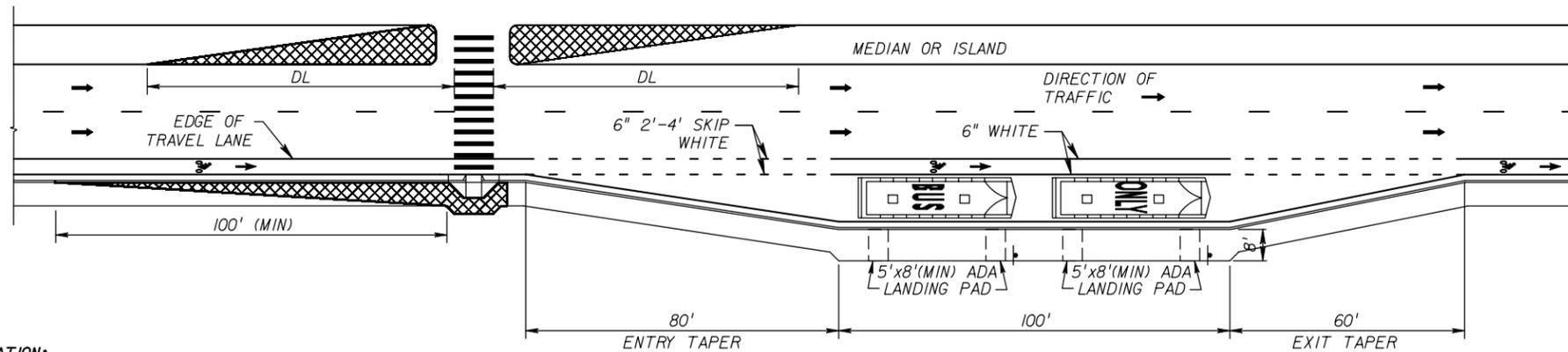
NOTES:

- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE I-24.

M-3
MID-BLOCK BUS STOP ON A DIVIDED ROADWAY WITHOUT ON-STREET PARKING

CLEAR AREA (SEE FIGURE I-24 FOR DETAILS)

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS BAYS/STOPS	FIGURE I-26
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- AREA OF HIGH VOLUME OF VEHICLES AND/OR PASSENGERS.

CONDITIONS:

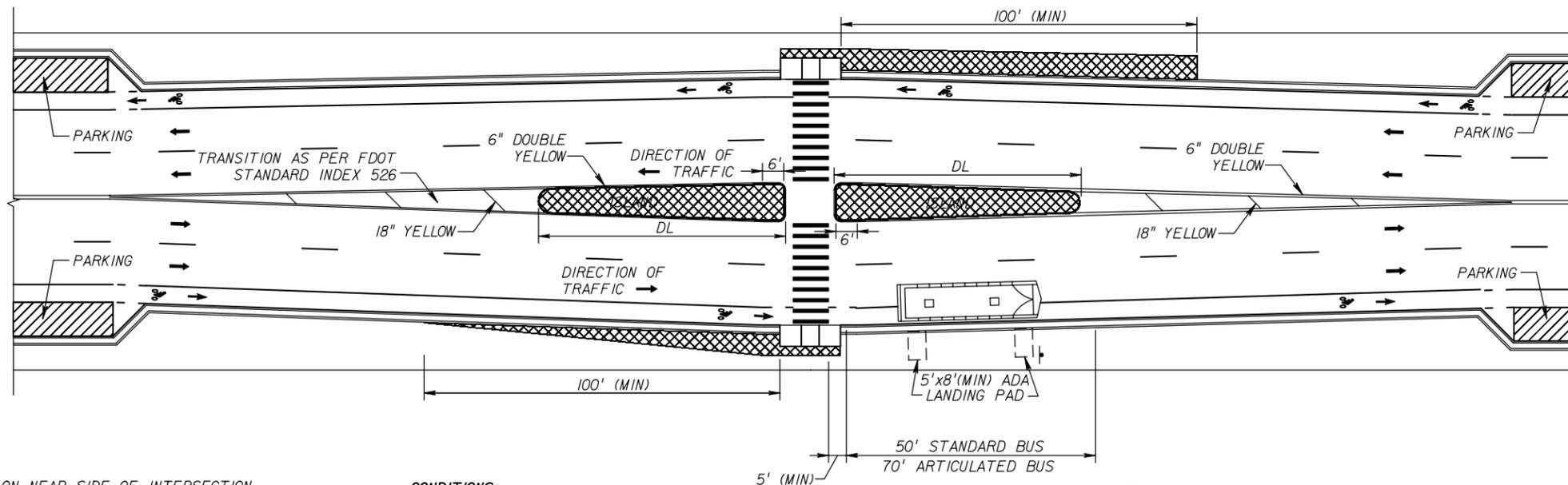
- ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:

- FOR BUS BAY DETAILS SEE FIGURE 1-8 & 1-9.
- BUS STOP AREA SHOWN FOR TWO BUSES.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE 1-24.

M-4

MID-BLOCK BUS BAY ON A DIVIDED ROADWAY WITHOUT ON-STREET PARKING



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK, LOW VOLUME OF VEHICLES AND STREET PARKING EXISTS.

CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:

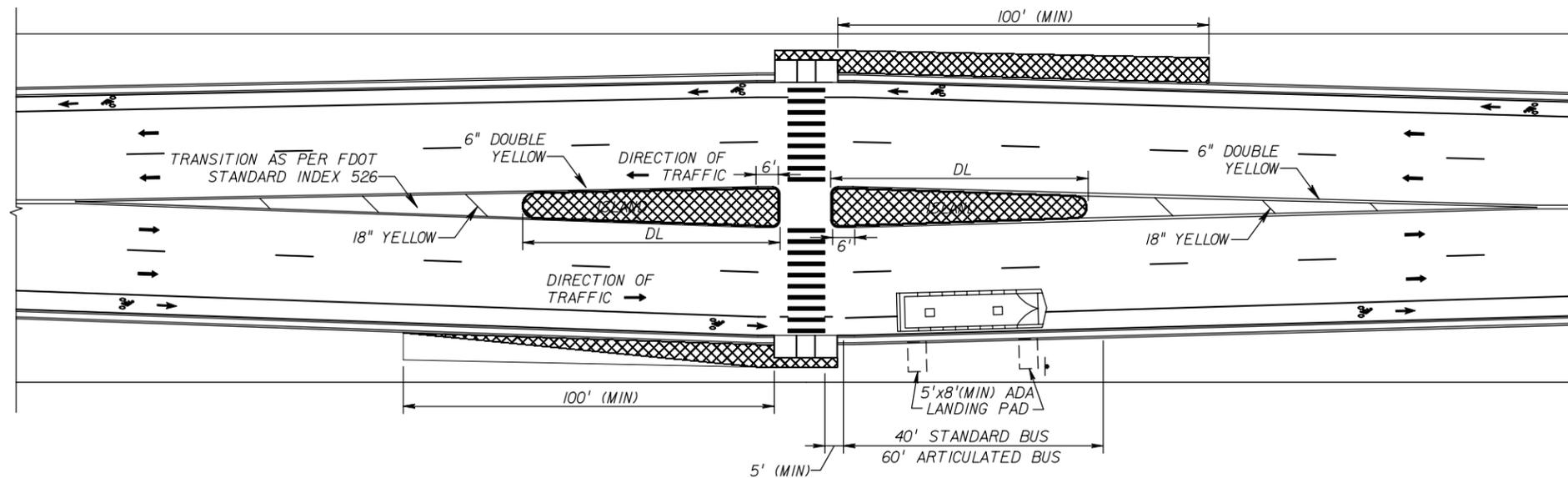
- INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE 1-24.

M-5

MID-BLOCK BUS STOP ON AN UNDIVIDED ROADWAY WITH ON-STREET PARKING

- PARKING AREA
- CLEAR AREA (SEE FIGURE 1-24 FOR DETAILS)

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS BAYS/STOPS	FIGURE 1-27
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

CONDITIONS:

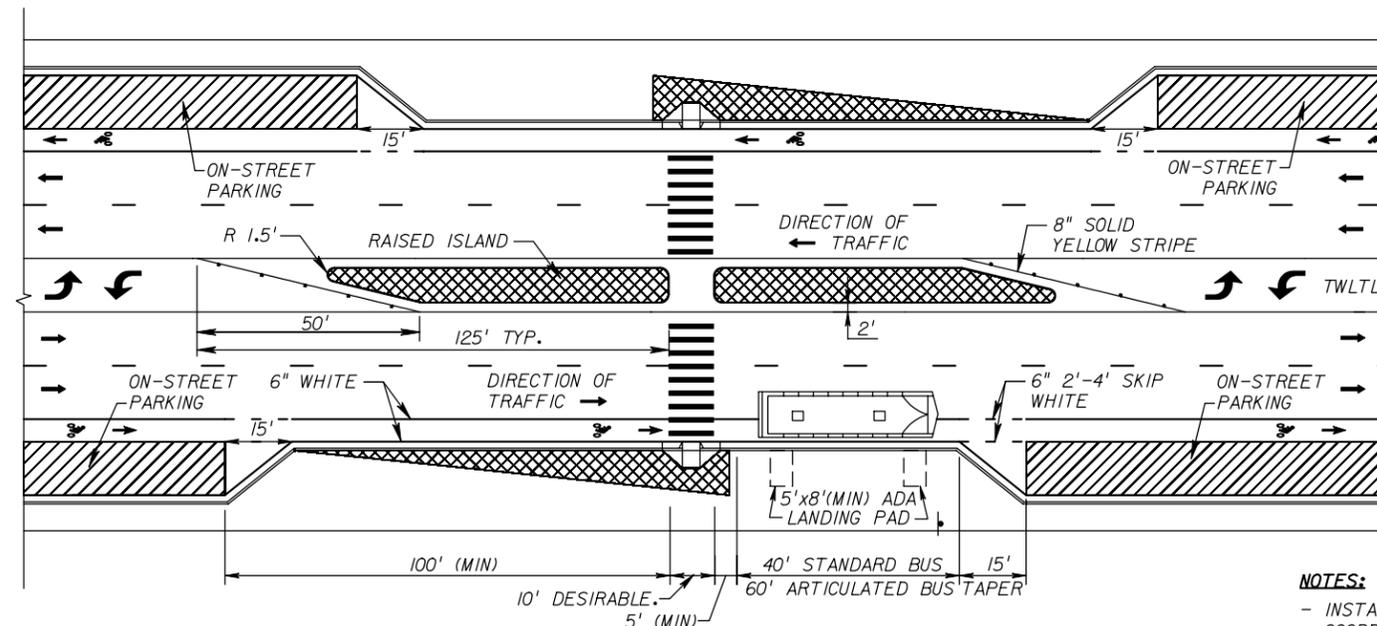
- ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:

- INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

M-6

MID-BLOCK BUS STOP ON AN UNDIVIDED ROADWAY WITHOUT ON-STREET PARKING



TYPICAL APPLICATION:

- AREAS WITH HIGH VOLUME OF PEDESTRIANS ON SIDEWALK SUCH AS CENTRAL BUSINESS DISTRICTS.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- ON-STREET PARALLEL PARKING.

CONDITIONS:

- ARTERIAL DESIGN SPEED \leq 45 MPH.

NOTES:

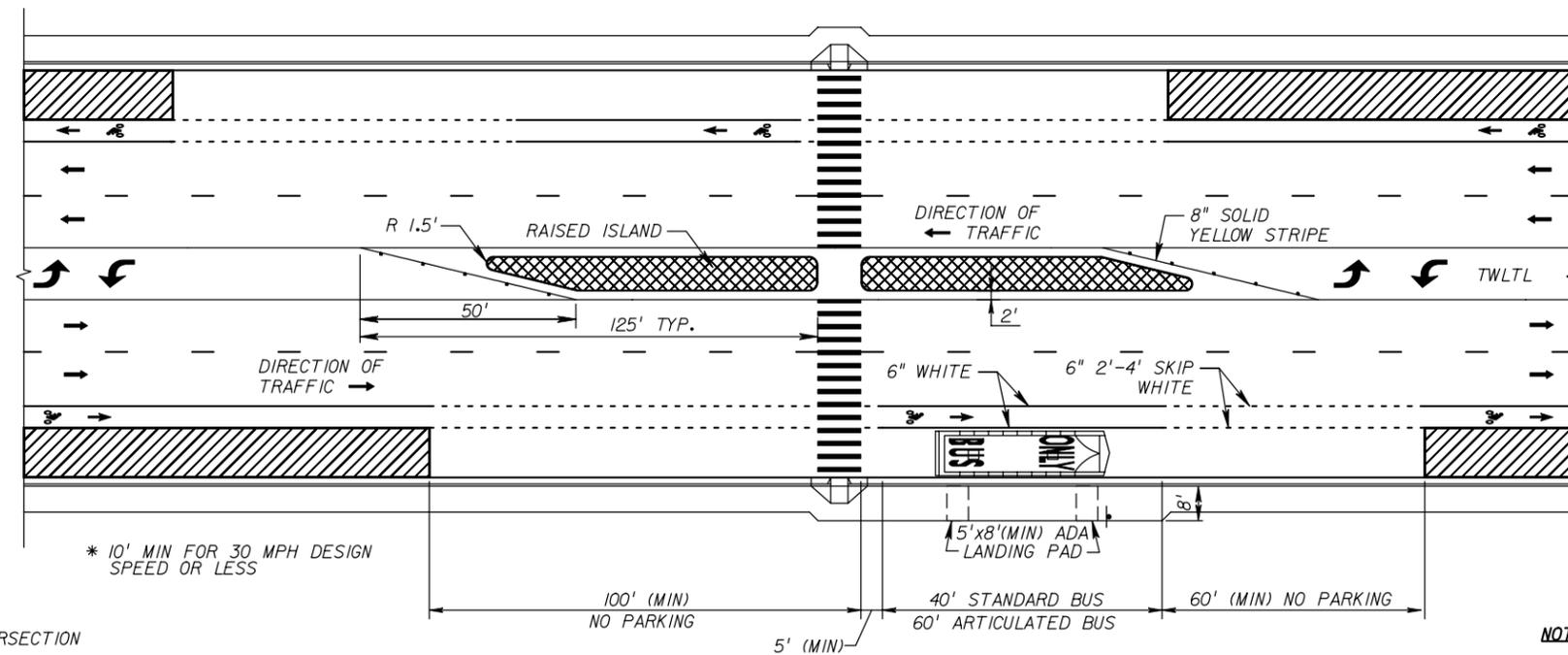
- INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE I-24.

- PARKING AREA
- CLEAR AREA (SEE FIGURE I-24 FOR DETAILS)
- TWTL TWO WAY LEFT TURN LANE

M-7

MID-BLOCK NUB/BULB WITH TWO-WAY-LEFT-TURN-LANE MEDIAN AND ON-STREET PARKING

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS STOPS	FIGURE I-28
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



TYPICAL APPLICATION:

- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND STREET PARKING EXISTS. WHEN PEDESTRIAN VOLUME IS HIGH, NUB ALTERNATIVE IS PREFERABLE.
- ON STREET PARALLEL PARKING.

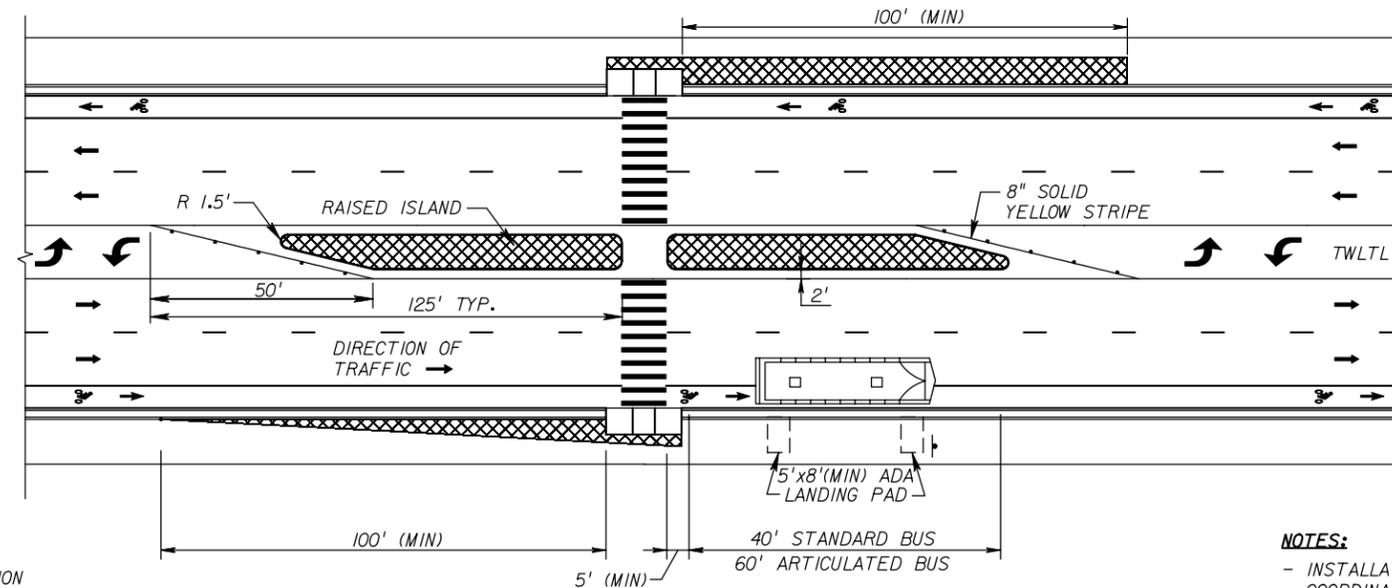
CONDITIONS:

- ARTERIAL DESIGN SPEED ≤ 45 MPH.

NOTES:

- INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

M-8
MID-BLOCK BUS BAY WITH TWO-WAY-LEFT-TURN-LANE
MEDIAN AND ON-STREET PARKING



PARKING AREA
 CLEAR AREA (SEE FIGURE 1-24 FOR DETAILS)
 TWLTL TWO WAY LEFT TURN LANE

NOTES:

- INSTALLATION OF MEDIAN ISLAND TO BE PROPERLY COORDINATED WITH ADJACENT PROPERTY ACCESS REQUIREMENTS (DRIVEWAYS AND SIDE STREETS).
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

TYPICAL APPLICATION:

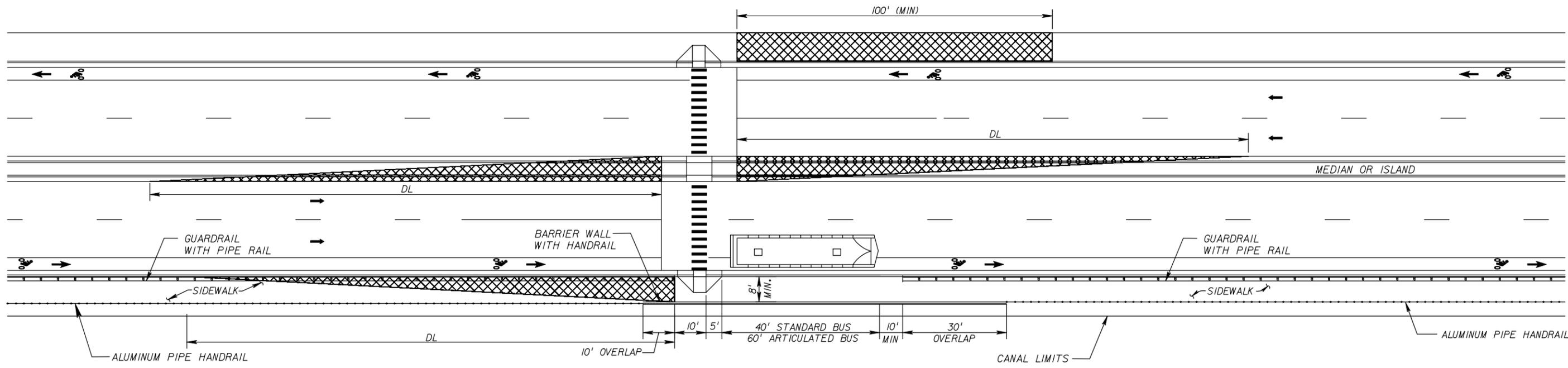
- RIGHT TURN BAY EXISTS ON NEAR SIDE OF INTERSECTION AND FAR SIDE BUS STOP CAN NOT BE PROVIDED.
- MID-BLOCK CROSSING NEEDED TO ACCESS A TRANSIT DEMAND GENERATOR.
- LOW VOLUME OF PEDESTRIANS ON SIDEWALK AND LOW VOLUME OF VEHICLES.

CONDITIONS:

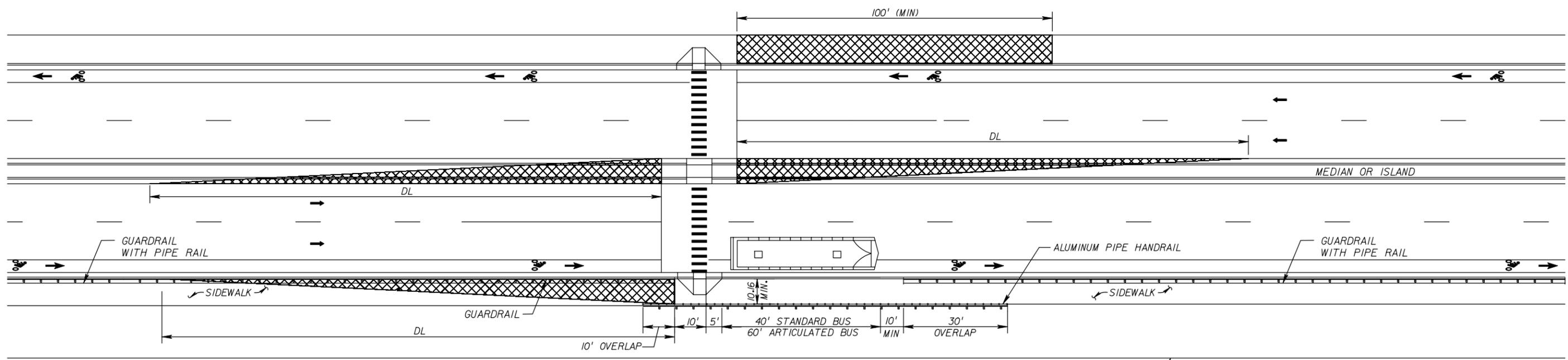
- ARTERIAL DESIGN SPEED ≤ 45 MPH.

M-9
MID-BLOCK BUS STOP WITH TWO-WAY-LEFT-TURN-LANE
MEDIAN WITHOUT ON-STREET PARKING

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITY GUIDELINES MID-BLOCK BUS BAYS/STOPS	FIGURE 1-29
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



MID-BLOCK BUS STOP ADJACENT TO CANAL BARRIER WALL OPTION



MID-BLOCK BUS STOP ADJACENT TO CANAL GUARDRAIL OPTION

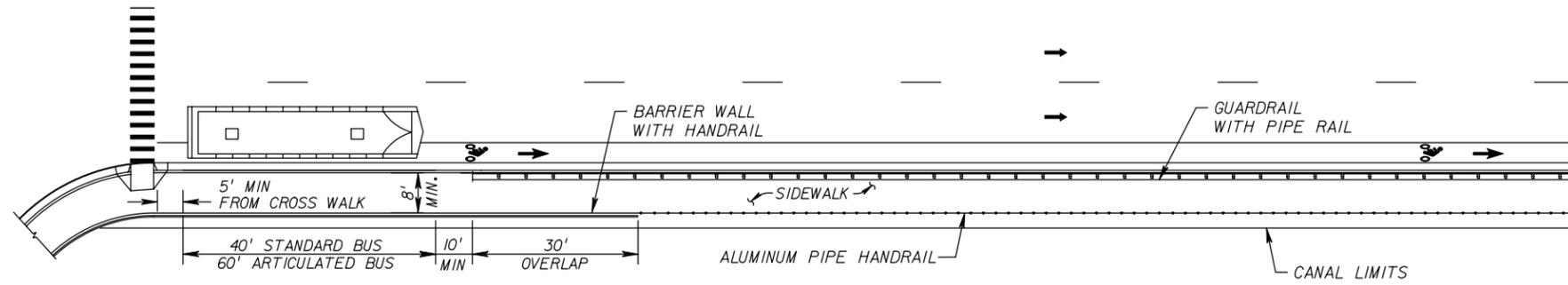
NOTES:

- SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
- SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
- BUS STOP AREA SHOWN FOR ONE BUS.
- SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE 1-24.

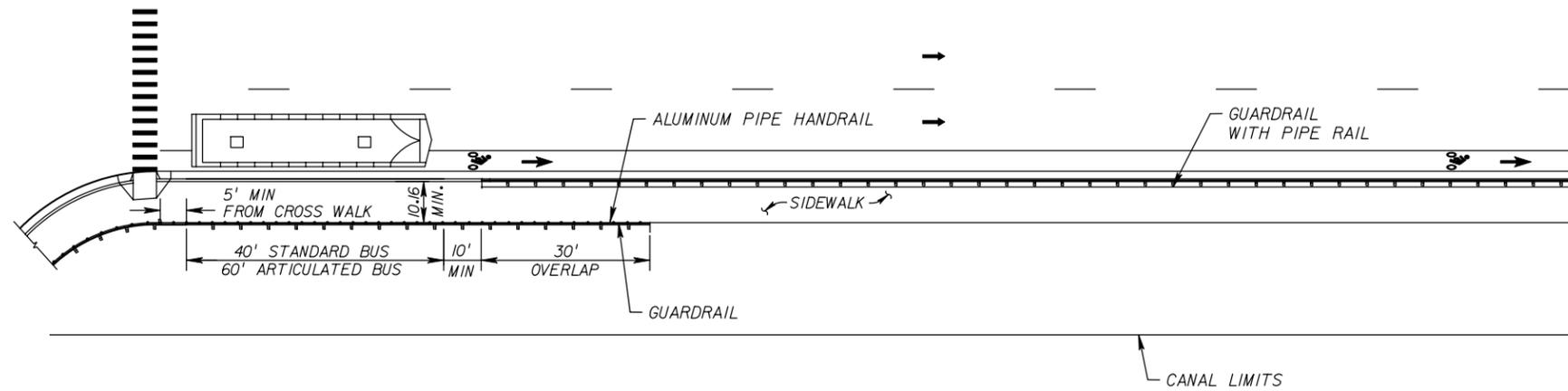
☒ CLEAR AREA (SEE FIGURE 1-24 FOR DETAILS)

FDOT PREFERRED

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION			TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION ADJACENT TO CANAL	FIGURE 1-30
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION	ROAD NO.	COUNTY	FINANCIAL PROJECT ID		



FAR SIDE BUS STOP ADJACENT TO CANAL BARRIER WALL OPTION



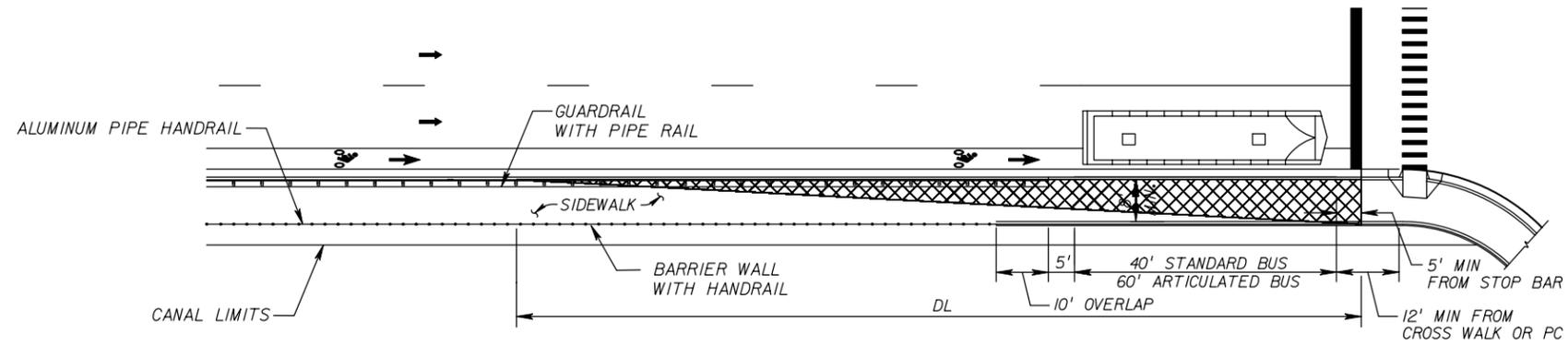
FAR SIDE BUS STOP ADJACENT TO CANAL GUARDRAIL OPTION

NOTES:

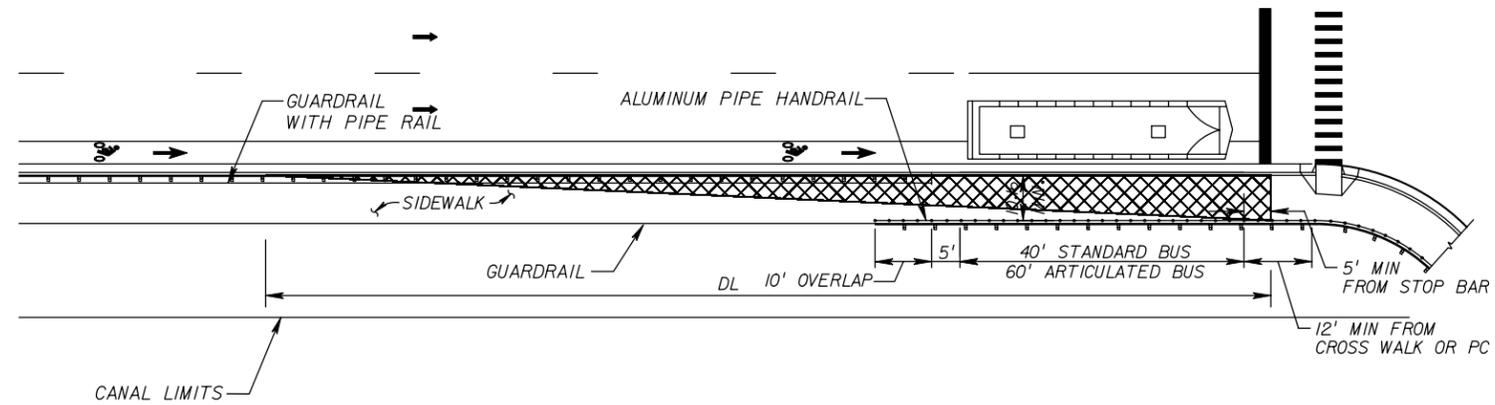
- SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
- SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

FDOT PREFERRED

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION ADJACENT TO CANAL			FIGURE
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION					1-31



NEAR SIDE BUS STOP ADJACENT TO CANAL BARRIER WALL OPTION



NEAR SIDE BUS STOP ADJACENT TO CANAL GUARDRAIL OPTION

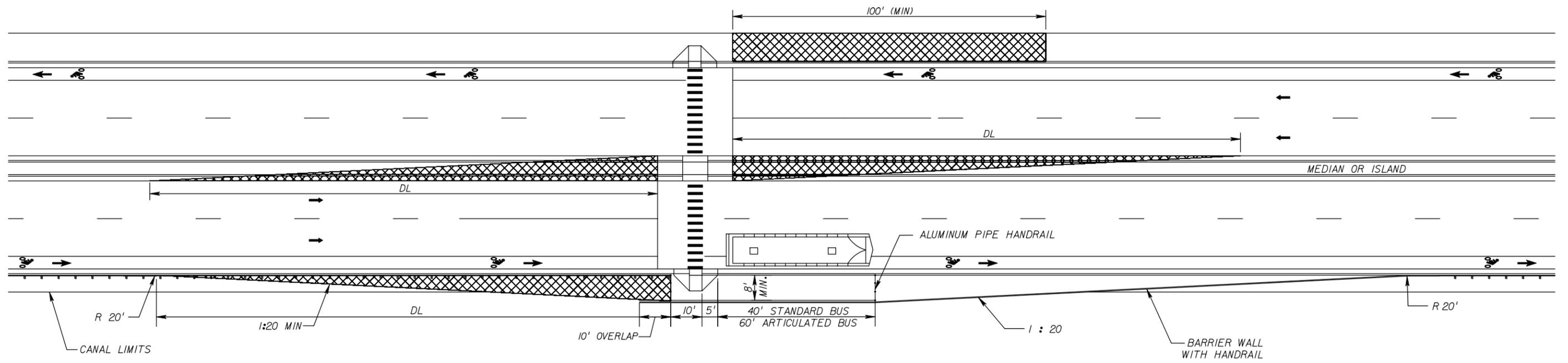
 CLEAR AREA
(SEE FIGURE I-24 FOR DETAILS)

NOTES:

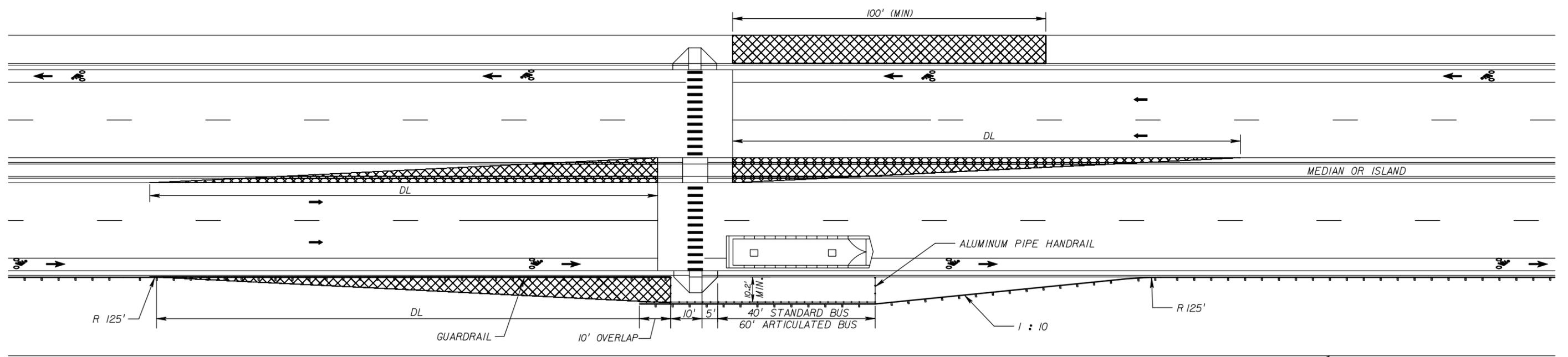
- SEE FDOT STANDARD INDEX I7346 FOR TRAFFIC CONTROL DEVICE DETAILS.
- SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE I-24.

FDOT PREFERRED

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION ADJACENT TO CANAL			FIGURE I-32
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION					



**MID-BLOCK ISOLATED BUS STOP
ADJACENT TO CANAL BARRIER WALL OPTION**



**MID-BLOCK ISOLATED BUS STOP
ADJACENT TO CANAL GUARDRAIL OPTION**

- NOTES:**
- SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
 - SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
 - BUS STOP AREA SHOWN FOR ONE BUS.
 - SEE "MID-BLOCK CROSSING" SECTION OF THE GUIDELINES FOR FURTHER DETAILS.
 - CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
 - DIMENSION "DL" CAN BE FOUND ON FIGURE 1-24.

☒ CLEAR AREA
(SEE FIGURE 1-24 FOR DETAILS)

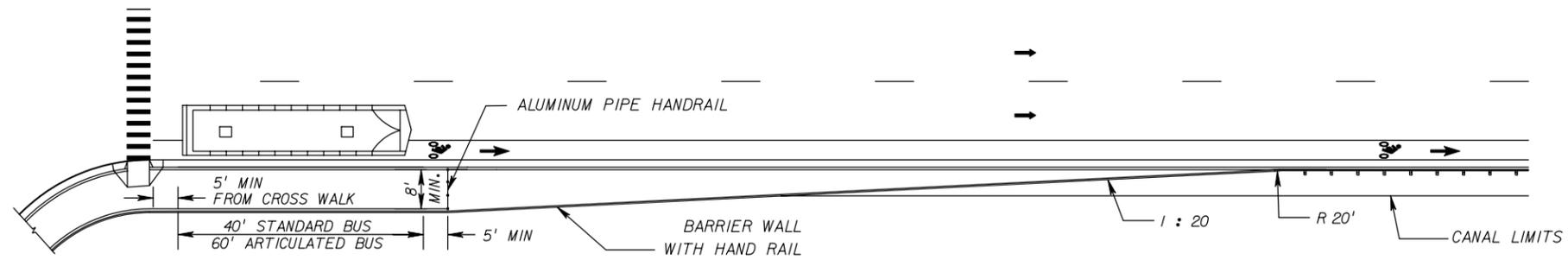
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REVISIONS					
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION

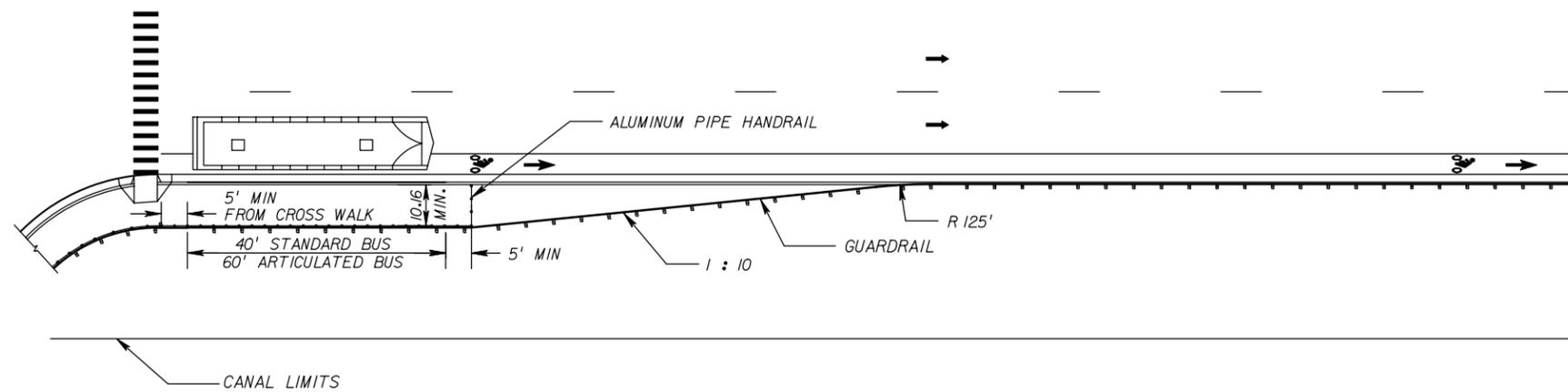
STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION		
ROAD NO.	COUNTY	FINANCIAL PROJECT ID

**TRANSIT FACILITIES GUIDELINES
BUS STOP LOCATION
ADJACENT TO CANAL**

FIGURE
1-33



FAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL BARRIER WALL OPTION



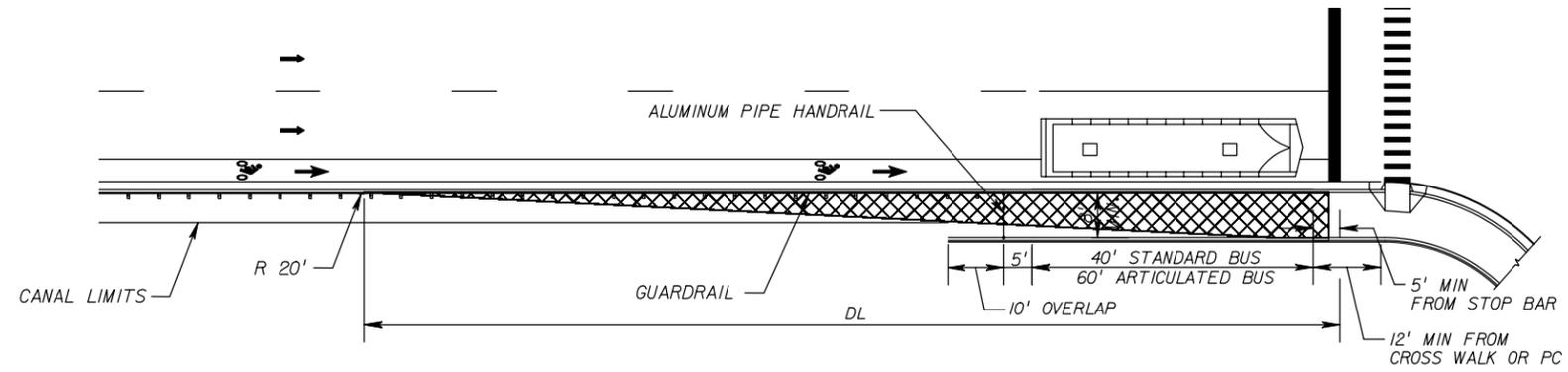
FAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL GUARDRAIL OPTION

NOTES:

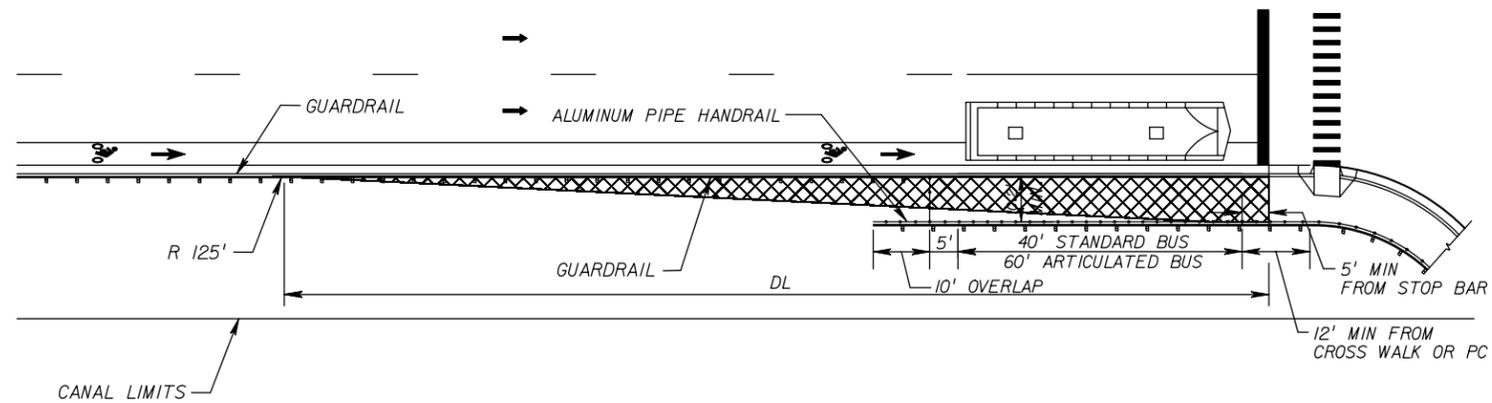
- SEE FDOT STANDARD INDEX 17346 FOR TRAFFIC CONTROL DEVICE DETAILS.
- SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.

FDOT PREFERRED

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION ADJACENT TO CANAL	FIGURE 1-34
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION			



**NEAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL BARRIER WALL OPTION**



**NEAR SIDE ISOLATED BUS STOP
ADJACENT TO CANAL GUARDRAIL OPTION**

☒ CLEAR AREA
(SEE FIGURE I-24 FOR DETAILS)

NOTES:

- SEE FDOT STANDARD INDEX I7346 FOR TRAFFIC CONTROL DEVICE DETAILS.
- SEE FDOT STANDARD INDEXES FOR DETAILS ON GUARDRAIL AND BARRIER WALL.
- BUS STOP AREA SHOWN FOR ONE BUS.
- CURB RAMP TYPE IS SHOWN ONLY AS AN ILLUSTRATION.
- DIMENSION "DL" CAN BE FOUND ON FIGURE I-24.

FDOT PREFERRED

REVISIONS						STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION	TRANSIT FACILITIES GUIDELINES BUS STOP LOCATION ADJACENT TO CANAL	FIGURE I-35
DATE	BY	DESCRIPTION	DATE	BY	DESCRIPTION			