

# DISTRICT THREE DESIGN NEWSLETTER



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April—June, 2013

## *From the Editor's Desk - Establishing R/W Requirements*

*Scott Golden, District Design Engineer*

District Three has been very fortunate to have received funding for construction on so many projects (SR 79, US 331, Choctawhatchee Bay Bridge (US 331), Cody Ave. at Hurlburt Field, Capital Circle, Pensacola Bay Bridge (US 98), SR 87, etc.). In addition, we have a large number of capacity projects under design. Our consultants should be working diligently to get accurate right of way requirements to our Survey and Mapping Office. We need to do everything that we can to get these projects ready, and the critical path is almost always right of way acquisition.



Establishing correct right of way requirements on a project must involve a multi-disciplinary team that includes the Engineer(s) of Record (EOR), Survey and Mapping staff, Right of Way staff, Design staff and the Design Project Manager. Communication and coordination is, as always, the key to success. Every project is different and has its' own set of unique challenges, but most projects have several things in common: it will either be rural or urban and almost always includes pond sites. As you know, the urban projects are typically the most expensive for right of way and often impacts businesses. On the other hand, the rural projects are typically high speed and require more right of way to accommodate the wider typical sections. In many cases, this is complicated and compounded by the District's rolling terrain.

I am working with our Right of Way and Legal staff as an expert witness for Order of Takings (OT's). On average, I spend between four (4) and eight (8) hours preparing for every OT (many of which are settled a day or two before the hearing). The preparation is basically the same: review the right of way maps, plans (plan, profile, cross sections, maintenance of traffic and phasing), Pond Siting Reports, if applicable, and field review the parcels. The purpose of the review is to answer one question: "Does the Florida Department of Transportation need this property to build this facility?"

The EOR should review every required parcel individually. Establishing the correct right of way requires good judgment in conjunction with your team. A consistent right of way width is the most desired but must be balanced with cost of the land, impacts to property owners, constructability and maintainability. This holds true for pond sites. Evaluation of alternate pond sites and a good justification in the "Pond Siting Report" is paramount for each selected/ proposed pond. In the end, the EOR must be able to answer the following question with a "yes," "Does the FDOT need this property to build this facility?" Please ensure that when you show a required right of way line that you can defend that decision. The key to being defensible with your right of way requirements is coordination and communication with the FDOT staff while using good engineering judgment.



### *District III Quarterly Design Newsletter*

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## Top Ten Quality Control Comments April – June, 2013

1. Effective January 2014 the thermoplastic will be included in construction contracts for projects with construction time of less than 365 days. At present the District includes thermoplastic in maintenance contracts. This determination was made to comply with the Roadway Design Bulletin 13-06.
2. Ensure that design is not calling for Milling/Resurfacing of paved side streets beyond the return radius/right of way (no greater than 50')
3. Ensure that Temporary Rumble Strips ( pay item 102-910) are being included into project designs when they are out of service during construction.
4. When utilizing Pay Item 0102-14, there needs to be a note in the plans to indicate that a Traffic Control Officer will be necessary. Refer: Estimates Bulletin 11-05
5. If Lump Sum Pay Item 0710-90 is being used for a project, Pay Item 0706-3 in TRNS\*PORT is not necessary; these items are included in the cost for Pay Item 0710-90.
6. When proposing a bike path between a right turn lane and a thru lane , ensure that the bike lane is 5 foot wide. Refer: Design Standards, Index 17347
7. Irrigation/Landscape plans need to refer to Design Standards, Index 591, when directional boring is to be utilized for placing irrigation sleeves under the existing roadway.
8. Pay items have not been included as indicated in the Indices which have been called out in the Traffic Control Plans.
9. General Notes should be updated for each individual project. Not all notes apply to all projects.
10. Include the "goes with" note on the Key Sheet when projects are being together as a goes with.

### Design Spotlight — Ray Corbitt Project Manager

Scott Golden, P.E., District Design Engineer



Ray began his FDOT career in 2008. For the last 5 years, Ray has worked in Roadway Design. Ray and his wife Kayla have three children (Landen 9, Lawson 6, and Tinsley 2). He enjoys spending his free time with family and friends. Ray has recently accepted a position in the Project Management section of the District Three Design office.

Please join me in congratulating Ray and wishing him success as he moves into his new role as a Design Project Manager.

*During school a professor gave us a quiz. When reaching the last question it asked: "What is the first name of the woman who cleans the school?" Surely this was a joke . I had seen the cleaning woman several times, but how would I know her name? I handed the paper in, leaving the last question blank. Before the class ended, one student asked if the last question counted toward our grade. "Absolutely," the professor said. "In your career, you will meet many people. All are significant. They deserve your attention and care, even if all you do is smile and say hello." I've never forgotten that lesson. I also learned her name was Dorothy. - Joann C. Jones*

## *Supplemental Agreement Report – April – June 2013*

Keith Hinson, P.E., District Value Engineer/QA/QC Manager

Following is a sample of Supplemental Agreements for the second quarter of 2013 (April and May). The category of Supplemental Agreements that are included in this summary are 126 and 101. This summary is included in the Quarterly Design Newsletter as a tool to inform designers of errors and omissions that can lead to Supplemental Agreements and unnecessary cost to the public. Below are brief descriptions of those errors or omissions and the Department's responses.

**Description Code: 126: Computation error.**

**Reason:** Provide for additional borrow material for correcting drop-off conditions of the existing shoulders, and providing for additional build up along the roadway and shoulder areas for the project.

**Granted Time: 0 Days**

**Increase: \$ 103, 549.24**

**Response: Avoidable: No action recommended.**

**Description Code: 101: Necessary Pay Items not included in the contract.**

**Reason:** Add pay item 0102-71-24 (Barrier Wall, Temporary, Relocate, Type K) and pay item 0104-1 (Artificial Coverings/Rolled Erosion Control Products) to the projects. Artificial Coverings for erosion control. Due to the limited grassed shoulder and slopes between the roadway and adjacent bodies of water throughout the project limits, storm water runoff cannot be contained satisfactorily with the use of sediment barriers alone. It was determined that the use of artificial coverings would allow storm water to flow over the embankment material and would significantly limit the loss of embankment and/ or silt into the adjacent bodies of water. During review of the phasing and MOT plans, it was determined that temporary barrier wall would need to be relocated during different phases of the project to allow ingress and egress of Contractor's equipment, supply deliveries, etc. Due to the limited right-of-way throughout the project limits, access to the construction zones is limited due to necessary temporary barrier wall.

**Granted Time: 0 Days**

**Increase: \$ 91, 423.00**

**Response: Unavoidable: No action recommended.**

## *Engineering and Operations Memorandum—13-02*

Brian Blanchard, P.E, Assistant Secretary for Engineering and Operations

**ENGINEERING AND OPERATIONS MEMORANDUM 13-02**

**DATE:** June 18, 2013

**FROM:** Brian Blanchard, P.E., Assistant Secretary for Engineering and Operations

**TO:** Duane Brautigam, Jim Spalla, Mark Wilson, David Sadler, Tim Lattner, Tim Ruelke, Darryll Ockstader, John Lanham, Trey Tillander, Juan Flores, Bob Romig, District Secretaries, District Directors

**COPIES:** Tom Byron, Lora Hollingsworth, Brian Peters, Richard Biter, Michael Shepard

**SUBJECT:** BUSINESS LOGOS ON FDOT DOCUMENTS



BACKGROUND

In order to provide a uniform approach in the placement of business logos on or within documents developed for FDOT, consultant business logos are no longer permitted on plans, documents, reports, presentations, etc. This will provide more consistency, predictability and repeatability to the way Department documents are developed and processed across the state.

REQUIREMENTS and IMPLEMENTATION

Effective immediately consultant business logos will not be allowed on any material produced for or on behalf of the Department. The Districts are expected to ensure this policy is followed through communication with consultants performing work and through Quality Assurance processes.

# CADD TRICKS, TIPS, UPDATES

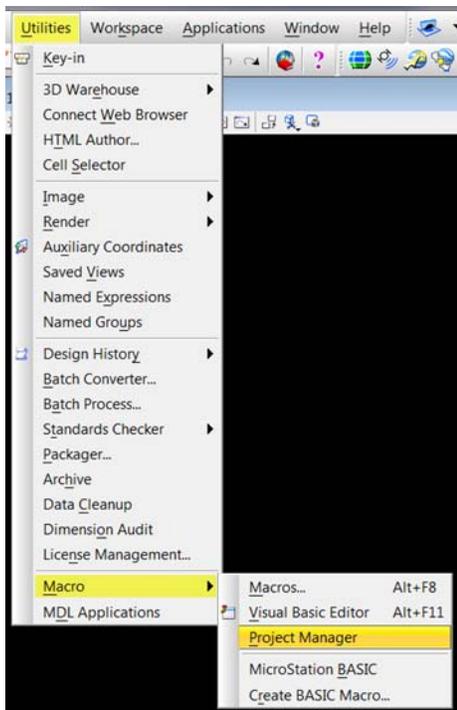
Howard Helms, CADD Manager ; Kenny Rudd, Senior Roadway Design CADD Specialist

## How do we get the secondary unit of measure on those computation booklet sheets using Quantity Manager ?

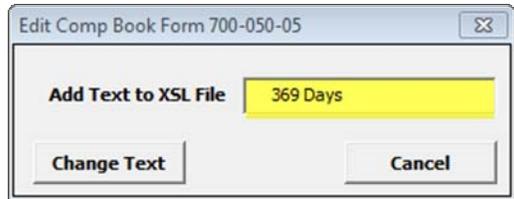
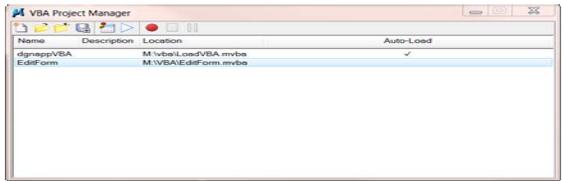
Geopak's Quantity Manager uses a style sheet to create forms. The style sheets are located in the ....Classes\samples\standards\reports directory. They are written in *xml* which is similar to *html* in nature. A user can edit these xml files and insert the secondary unit of measure in the proper place, and it will print on the form. A VBA is available which will do that for you.

While in MicroStation on main toolbar:

UTILITIES > MACRO > PROJECT MANAGER



Go to folder icon on toolbar **LOAD PROJECT**  
 Navigate to <\\FDOT2010\VBA\EDITFORM.MVBA>  
 Highlight editform.mvba and run  Macros



When the comp book sheet is run from QM the proper secondary unit of measure will print on the form. Make sure to run the macro a second time after producing the sheet in order to set the xslt file back to it's original status.

STATE OF FLORIDA, DEPARTMENT OF TRANSPORTATION		Page No. 1
LUMP SUM QUANTITIES		FORM 700-050-05 CONSTRUCTION 05/02
		PAY ITEM NO. 2401515/03647
PAY ITEM DESCRIPTION <u>Sidewalk Concrete (3Y48A)</u>	DESIGNER QUANTITY (NO SECONDARY UNIT) _____	
PAY ITEM DESCRIPTION _____	CONSTRUCTION FINAL QUANTITY _____	
	DESIGNER QUANTITY (WITH SECONDARY UNIT, SEE BELOW) <u>369 Days</u>	
	CONSTRUCTION FINAL QUANTITY _____	
SECONDARY UNIT COMPUTATIONS		