

DISTRICT THREE DESIGN NEWSLETTER



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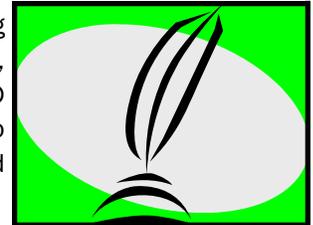
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From the Editor's Desk - Digital Delivery of Plans Miranda Glass, District Roadway Design Engineer

For those of you who may not have heard, a new process for signing and sealing project plans and documents (specifications, computation books, etc.) is being written into the upcoming CADD Production Criteria Handbook. This new process is being referred to as “Digital Delivery” (formerly “Streamlined Electronic Delivery”) and has been used in a few pilot projects so far within the State.



The existing methodology, now called “Classical Electronic Delivery”, required production of an index and postscripts of the plans and utilized EDI software to create a manifest and signature sheets. The signature sheets were then physically signed and sealed after verification of the project. By signing the signature document, the signatory is electronically signing all files listed in the signature file.

Digital Delivery has some very distinct differences to the Classical Electronic Delivery method. In this case, no index is produced. A document is created with all plan sheets in index order in Adobe Portable Document Format (PDF) which is signed and sealed with a certificate based Digital Signature(s). This same is true for the specifications PDF and other documents requiring the professional’s seal. Digital Delivery is set up in a way that the Engineer is signing and sealing the actual document being viewed.

The type of Digital Certificates used for FDOT work should at a minimum meet the Federal Government’s Access Certificates for Electronic Services (ACES) program. A webinar video posted by the FDOT’s Engineering CADD Systems Office (ECSO) provides more detail on the process as well as some approved providers for Digital Certificates. The video also gives a general overview and glimpse of the steps included in a typical Digital Delivery. The video is located at the link below. Be sure to check it out!

<http://www.dot.state.fl.us/ecso/downloads/clips/Videos/IntroductionToDigitalDelivery.wmv>



District III Quarterly Design Newsletter

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Top Ten Quality Control Comments Jan. – March, 2013

1. Designers are proposing new 15" side drain and cross drain pipes. Sections 3.10.1 and 4.10.4.1 of the Drainage Manual states to use a minimum diameter of 18". Side drain and cross drain pipe sizes should be 18" or greater except for extension of an existing pipe.
2. Designers are proposing 4" thick concrete to be placed across driveways. According to the Design Standards, Index 515, sheet 2 of 7, 6" concrete thickness should be used in these areas.
3. Voltage for Overhead Electric Lines should be labeled, especially in areas where heavy equipment will be utilized.
4. Due to the retirement of Don Boutwell, his name needs to be replaced in applicable plans with the title "Area Utility Manager".
5. As required, the quantity for each Pay Item in Trns*port is to match the quantity in the Computation Book and the associated Summary Box in the Plans.
6. For 3R projects, existing front slopes 1:3 or flatter may remain within the clear zone as per 2013 PPM Volume I, 25.4.9.
7. Provide the FDOT Project Manager's name and phone number is on the Key Sheet below the Length of Project Box.
8. When certain Pay Items are specified to have a different Secondary Unit of Measure in the Basis of Estimates Manual, the quantity for the Secondary Unit of Measure is to be entered on the Computation Book Sheet and in Trns*port (e.g. Maintenance of Traffic, Clearing & Grubbing, etc.).
9. Variable milling and overbuild details should include information similar to that shown in 2013 PPM, Volume 2, Chapter 6, Exhibits (8A,9A,12A,13A,13B).
10. Make sure that site distance is not an issue on projects where fence, guardrail or handrail is being installed near turnouts.

Design Spotlight — Jonathan Harris District Utility Manager

Scott Golden, P.E., District Design Engineer



Jonathan has been employed with the Department since 2004, serving the last six years as the District Safety Program Manager in Planning and Traffic Operations. Prior to joining the Department, Jonathan worked with West Florida Electric Cooperative for 10 years, Phillips and Jordan for 3 years as a utility contractor, and a couple years with local machine shops. Jonathan graduated from Gulf Coast in 1991 with a degree in Electronics Engineering Technology.

Jonathan enjoys spending time with his family, wife Rebecca and daughter Samantha. In his spare time he enjoys adding to his collection of over a dozen pieces of antique tractors and equipment and their restoration. Please join me in congratulating Jonathan and wishing him success as he moves into his new role as the District Utility Administrator.

Human kindness has never weakened the stamina or softened the fiber of a free people. A nation does not have to be cruel to be tough.

~ Franklin D. Roosevelt

Supplemental Agreement Report – December – February 2013

Keith Hinson, P.E., District Value Engineer/QA/QC Manager

Following is a sample of Supplemental Agreements for the first quarter of 2013 (December through February). The category of Supplemental Agreements that is included in this summary is 113. This summary is included in the Quarterly Design Newsletter as a tool to inform designers of errors and omissions that can lead to Supplemental Agreements and unnecessary cost to the public. Below are brief descriptions of those errors or omissions and the department's responses.

Description Code: 113-Modification to pavement design required.

Reason: Extra work to place overbuild asphalt on the existing paved shoulder; and place borrow material to level and raise the grassed shoulder to remove the drop off condition.

Granted Time: 20 Days

Increase: \$118, 598.41

Response: Avoidable; cost to perform the referenced work is not considered higher than if it had been included in the original contract; therefore, no premium costs are recognized .

Hope makes today possible and tomorrow attractive, or at least less threatening. Hope makes death bearable...hope is the mainstay of our energy. We go forward –because we hope.

Arthur Jones

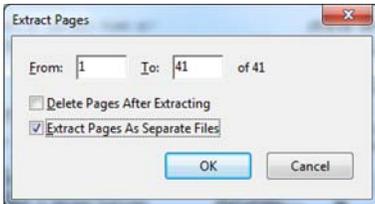


CADD TRICKS, TIPS, UPDATES

Kenny Rudd, Senior Roadway Design CADD Specialist

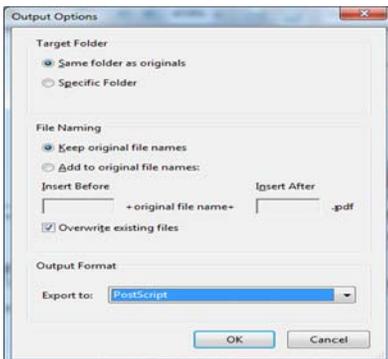
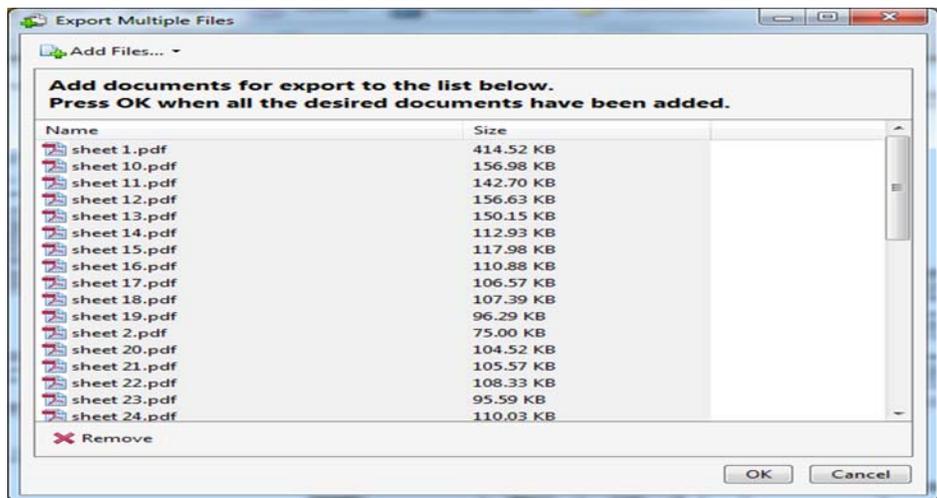
Create PostScript files from a PDF

If any of you are having difficulty relating to the creation of PostScript files that don't print the same as your PDF's, here are a few tips to help you create PostScript files directly from your PDF.



Open PDF file with Adobe
 Rename project file to **Sheet (space). pdf**
 Click **Document > Extract**
 Check **"Extract Pages as Separate Files"**

Click **File > Export Multiple Files**



There are two PostScript options.
 Choose **PostScript** not **Encapsulated Postscript**

In the directory you will need to rename pages (files) 1 thru 9 to "Sheet 001" thru "Sheet 009"
 Then rename "sheet 10" thru "Sheet 99" to "sheet 010" thru "Sheet 099"

The PostScript files will then look exactly like our PDF files.

