

# DISTRICT THREE DESIGN NEWSLETTER



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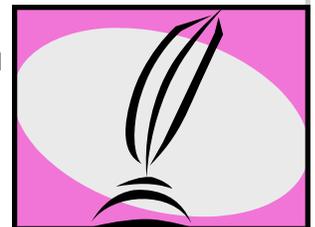
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July—September, 2013

## *From the Editor's Desk - Life From Another Vantage Point*

*Miranda Glass, District Roadway Design Engineer*

I recently was given the opportunity to serve in the Central Office as the Interim State Quality Assurance Administrator in the Roadway Design Section. During my stay, I was fortunate enough to touch and become involved in all of the sections within Roadway Design in one form or another. In life, we rarely get the opportunity to experience the world from another vantage point. However, during my three month tenure, I gained my “fly on the wall” understanding.



My first observation centered around the rush and pressures associated with the preparation and implementation of the 2013 Design Expo while simultaneously pushing to publish the July versions of the Design Standards and Plans Preparation Manual. This buzzing was very familiar, as Production is what we do every day. I could easily relate to the deadline and last minute changes to assure that all of the “t’s” have been crossed and “i’s” dotted. And then I was able to witness the familiar sigh of relief at their conclusion.

My interest was peaked by the many activities and research projects that were going on behind the scenes. From determining failure mechanisms of drainage pipes of certain materials to forecasting pavement life based on pavement layers and historical data, there was something happening in each discipline of Roadway Design. One of my colleagues made an astute statement that his job was to evaluate and consider the impacts of potential changes or materials and provide the tools to the districts so that they may focus, rightfully, on production.

What took me somewhat by surprise, although I’m not sure why, was the frequency and constant nature in which requests for information flooded in from all directions. The care and attention that each request was given was remarkable. Each word in response was scrutinized for all conceivable ways it could be misconstrued, misinterpreted, or misdelivered. There was a distinct understanding that any correspondence coming from the Central Office would be considered the official stance of the Department. What’s more, if you get it wrong, the effects are statewide.



*District III Quarterly Design Newsletter*

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## Top Ten Quality Control Comments July – Sept., 2013

1. Consultant business logos shall not be placed on plans. ( ref: Engineering and Operations Memorandum 13-02 )
2. “PMA” is to be included with “PG 76-22” in the Typical Section pavement designs for the structural course and the friction course asphalt pavement.
3. When the designer calls for the construction of a proposed traffic island, instructions must be provided as to the desired type. (concrete or sod)
4. Ensure that shoulders in the medians have a standard slope rate of 0.05 refs: P.P.M Vol. I , Chapter 2, Tables 2.3.1 and 2.3.2
5. For Pay Item 0514-71-1, the cost of filter fabric is to be included in the costs of the ditch pavement. Reference : Specification 530-5-4.
6. Ensure that manholes / valve boxes and gas valves that exist within the project limits are plotted and those identified for adjustment called out in the plans /summary box.
7. Ensure that the MOT and plans address any drop off hazards that are adjacent to a pedestrian or bicyclist travel way. Drop off hazards shall be protected in accordance with Index No. 600, Sheet No. 10 and P.P.M Vol. I , Section 8.8
8. Ensure that the date for the Design Standards on the key sheet is updated.
9. Show limits of clearing and grubbing in the plans when pay item is provided.
10. Offset proposed channelizing islands the appropriate distance from the edge of the mainline. Islands should not be placed flush with the mainline outside travel lane. Ref: Florida Intersection Design Guide 2013, Section 3.11

### Design Spotlight — James Holland Survey Data Manager

Scott Golden, P.E., District Design Engineer



James Holland started his career with the Florida Department of Transportation in 1991 serving as a survey crew member on John Ferguson’s party in Pensacola. The Pensacola office was moved to Milton and James advanced to managing the Milton satellite office, along with its field activities when John retired. Early this year (2013), James accepted the Survey Data Manager position here in Chipley where he supervises and performs reviews of consultant and in house surveys and design plans.

James is extremely knowledgeable of the Electronic Field Book program and survey collection processes. He helps survey personnel set up and run these programs. He is also very capable using Caice and is now involved with getting procedures ready for the use of Civil3d for surveying work program projects.

He is married, to a lovely lady named Vicky (for 20+ years). He has a son, two daughters and a son-in-law; and just a couple of months ago, got to experience the excitement and thrill of becoming a Grandfather to an adorable little girl.

*In honor of those fighting breast cancer and those who have been affected by breast cancer—this edition is pink for Breast Cancer awareness month (October).*

## ***Supplemental Agreement Report – June, July, August 2013***

Keith Hinson, P.E., District Value Engineer/QA/QC Manager

Following is a sample of Supplemental Agreements for the third quarter of 2013 (July– August). The category of Supplemental Agreements that are included in this summary are 126 and 101. This summary is included in the Quarterly Design Newsletter as a tool to inform designers of errors and omissions that can lead to Supplemental Agreements and unnecessary cost to the public. Below are brief descriptions of those errors or omissions and the Department's responses.

### **Description Code: 117 – Access Management Issues**

**Reason:** An additional southbound left turn lane, for a business' request, was constructed so southbound traffic especially tractor trailers would not have to proceed further south and make a u-turn. The Department evaluated and approved this access modification.

**Granted Time:** 0 Days

**Increase:** \$87, 039.34

**Response:** Unavoidable: No remedial action required.

### **Description Code: 001- Subsurface material or feature is not shown in plan.**

**Reason:** Use Granular Subbase (Limerock only 6 inches) in lieu of Type B Stabilization, because shallow ground water conditions would adversely affect compaction efforts of Type B Stabilized Subgrade.

**Granted Time:** 10 Days (Time granted to perform unforeseen additional work)

**Increase:** \$147, 720.72

**Response:** Unavoidable: No remedial action required.

### **Description Code: 001- Subsurface material or feature is not shown in plan.**

**Reason:** Recurring slope failure at a Pond Site was tested and determined that Slope Reinforcement would be necessary. Provides for additional cost and time for the reinforcement of slope stability at a Pond Site.

**Granted Time:** 30 Days

**Increase:** \$147, 938.91

**Response:** Unavoidable: No remedial action required.

### **Description Code: 120 – Hazardous material encountered requiring contract changes.**

**Reason:** Provide concrete ditch pavement in areas where contaminated soil was removed to prevent contaminated groundwater from entering the ditch and to provide stability for the proposed longitudinal ditches where they junction with existing lateral swales along the Right of Way.

**Granted Time:** 0 Days

**Increase:** \$29, 887.00

**Response:** Unavoidable: No remedial action required.

### **CSI: Description Code: 305 – Cost Savings Initiative**

**Reason:** To delete temporary critical sheet pile, subsoil excavation, and embankment backfill and adding the construction of rigid inclusion CSV column soil stabilization and associated load transfer devices.

**Granted Time:** 0 Days

**Decrease:** \$663, 867.33

**Response:** Unavoidable: No remedial action required.

### **CSI: Description Code: 305 – Cost Savings Initiative**

**Reason:** Substitute temporary guardrail in lieu of Type K Barrier Wall in the Special Detour bridge area.

**Granted Time:** 0 Days

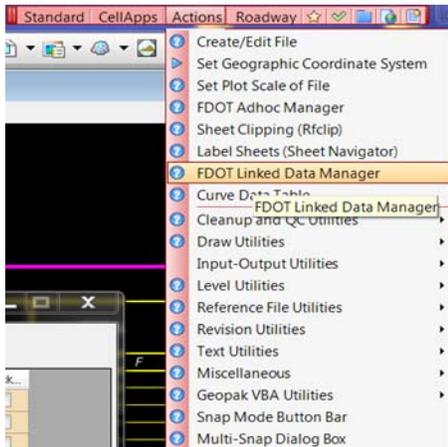
**Decrease:** \$15, 198.05

**Response:** Unavoidable: No remedial action required.

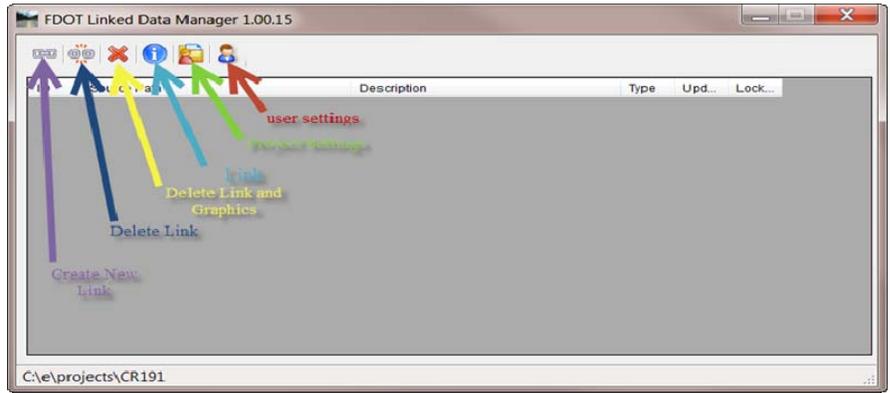
# CADD TRICKS, TIPS, UPDATES- FDOT Linked Data Manager

Howard Helms, CADD Manager ; Kenny Rudd, Senior Roadway Design CADD Specialist

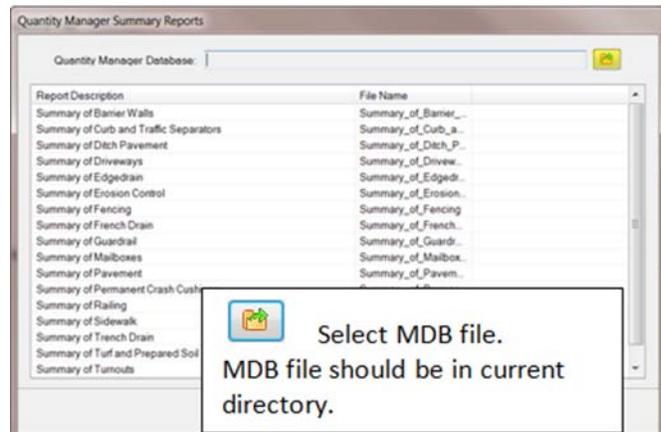
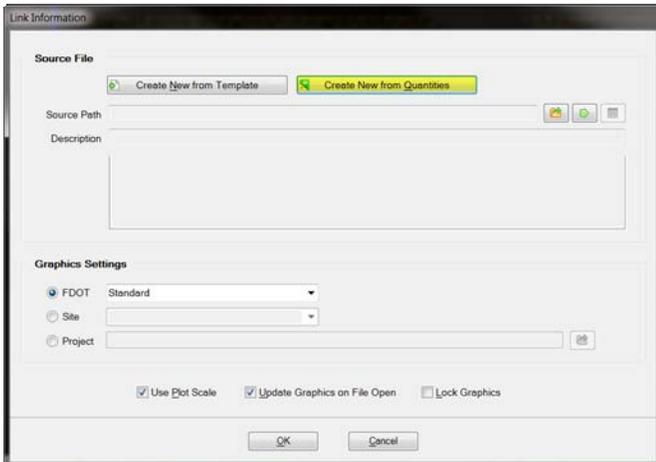
FDOT Toolbar, Actions – FDOT Linked Data Manager



Create New Link



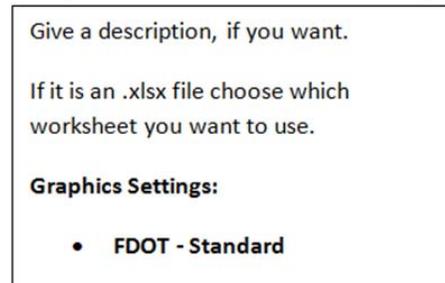
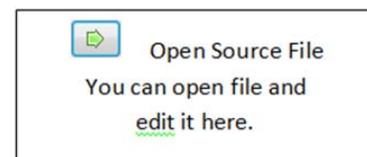
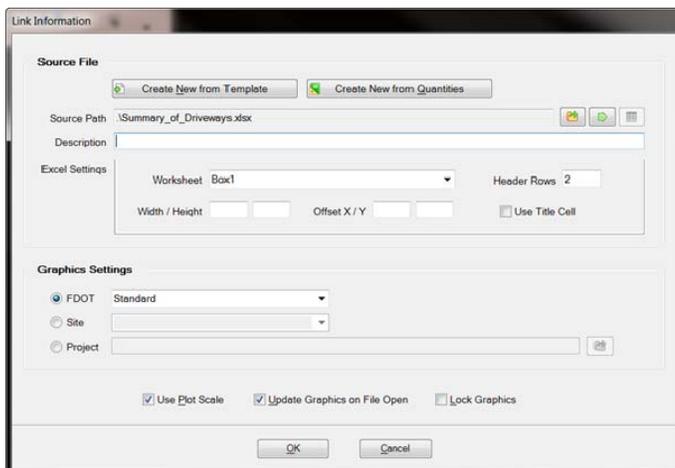
Create New from Quantities



There are various pre-made .xlsx files on the N: drive that comes with the FDOT toolbar.

Choose which one applies to you.

It will ask you to save it, save it in the project directory you are in.



After getting all settings click OK. It will ask for a data point in Microstation to place summary box. Left click where you want to place it.