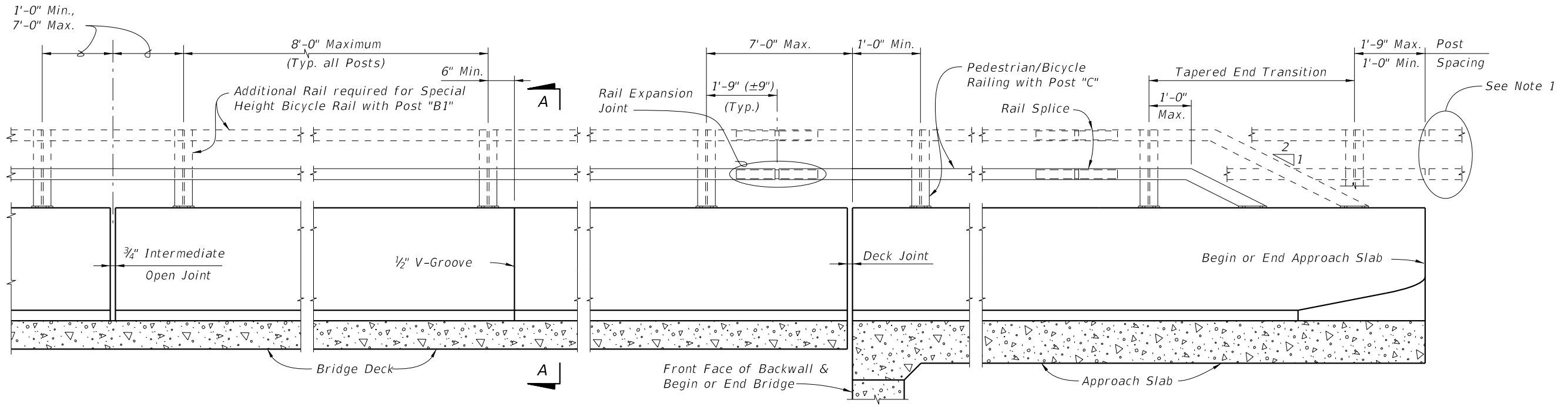
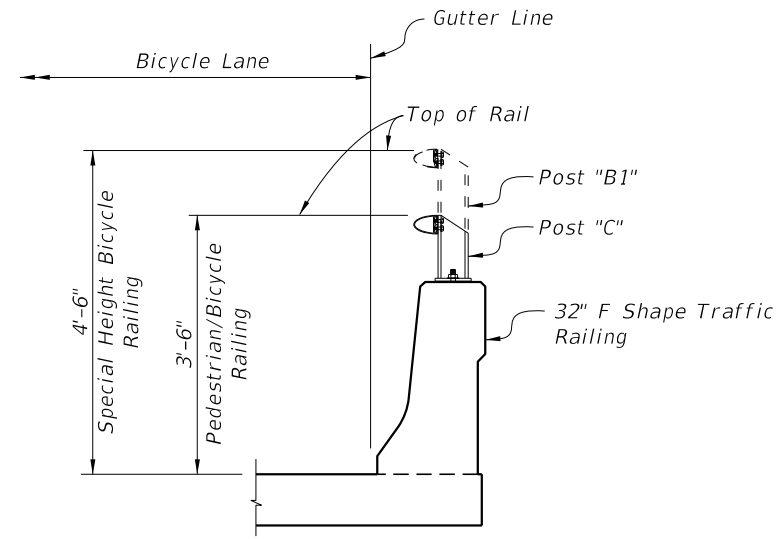


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ELEVATION OF INSIDE FACE OF TRAFFIC RAILING WITH PEDESTRIAN/BICYCLE BULLET RAILING




SECTION A-A  
TYPICAL SECTION THRU BRIDGE DECK  
(APPROACH SLAB SIMILAR)

NOTES:

1. Where Bullet Railing continues on retaining wall mounted Traffic Railings or other Traffic Railings, the Tapered End Transition shall be located at the terminus of the Bullet Railing.

CROSS REFERENCES:

- 1. For Post, Rail and Rail Expansion Joint fabrication and installation Details and Notes see Index No. 822.
- 2. For Traffic Railing Details, Reinforcement and Notes see Index No. 420.

LAST REVISION	REVISION	DESCRIPTION:		<b>FDOT DESIGN STANDARDS</b> FY 2012/2013	<b>BRIDGE ALUMINUM PEDESTRIAN/BICYCLE BULLET RAILING FOR TRAFFIC RAILING (32" F SHAPE)</b>	INDEX NO.	SHEET NO.
01/01/12						821	1