
Trends and Conditions

Special Report – November 2011

COMMUTING TRENDS IN FLORIDA

This special report of the FDOT Trends and Conditions series highlights commuting trends in Florida and the United States. Its basis is the 2010 American Community Survey (ACS) data. In Florida, 100,820 households and 7,417 people living in group quarters were surveyed in 2010.¹ In general, the trends reaffirm the continuing dominance of auto commuting but also suggest that fuel prices, economic pressures, and environmental concerns may be having some influence on travel behavior.

Florida's commuting trends can be summarized as follows:

- **Decreased auto availability** – Florida's zero-vehicle households increased for a third year to 7.0% in 2010, up from 6.2% in 2007, reversing the general downward trend. This compares with a national level of 9.1%.
- **Slight increase in commuting by driving alone** – In Florida, 79.9% of commuters drove alone, slightly above the 2009 level and 3.3% above the national average.
- **Smaller shares of commuting by carpool** – Commuting by carpool was 9.6% in Florida and 9.7% in the nation, down 0.8% from 2009 and 1.1% from 2002 in Florida.
- **Slightly more commuting by walking** – Commuting by walking was 1.7% in Florida and 2.8% nationwide. Florida's number is slightly up from 2009, while the U.S. number is similar to 2009.
- **Slight increase in transit use for commuting** – Transit use for commuting increased slightly to 2.1% in Florida and remained almost steady at 4.9% nationwide. Very slight increases from 2002 were observed statewide and nationally.
- **Slight decrease in work at home** – Working at home decreased slightly to 4.6% in Florida but remained flat nationally at 4.3%.
- **Slight increase in overall commute times** – Overall commute times increased slightly, resulting in the average one-way commute in Florida being 25.5 minutes, 0.2 minutes longer than the national average and below its 2005 peak.
- **Mobile work force** – In Florida, 18.6% of commuters work outside their county of residence, compared to 27.4% nationally.
- **No worker households** – Households with zero workers was 32.5% in Florida and nearly 27.2% in the nation, an increase in both from 2009.

While commuting is critically important in studying travel, it is only a portion of overall travel. Fuel price levels, the economic recession, individuals working from home, and no worker households all influence travel behavior. Although fundamental travel behaviors remain intact, the most notable changes in the data for recent years reflect the influence of the economy on several of the measures. The following tables and figures contain detailed information supporting the above summary.

¹ Group quarters include such places as college residence halls, residential treatment centers, skilled nursing facilities, group homes, military barracks, correctional facilities, and workers' dormitories.





Due to small sample sizes, many of the small differences over time and between locations may not be statistically significant. For information on data collection, sampling design, non-sampling error, and definitions, see

http://www.census.gov/acs/www/methodology/methodology_main/.

Table 1 presents Census and ACS results for Florida since 2002. Readers are encouraged to familiarize themselves with margins of errors by reviewing ACS guidance.

Table 1. Florida and U.S. Multi-Year ACS Trend									
	2002	2003	2004	2005	2006	2007	2008	2009	2010
VEHICLES AVAILABLE – FLORIDA									
No vehicles available	6.8%	7.0%	6.5%	6.6%	6.6%	6.2%	6.6%	6.6%	7.0%
1 vehicle available	40.9%	40.5%	40.3%	40.0%	39.4%	39.7%	40.5%	41.2%	41.1%
2 vehicles available	38.4%	38.7%	39.7%	39.2%	39.0%	38.8%	38.5%	38.3%	37.9%
3 or more vehicles available	13.9%	13.9%	13.5%	14.3%	15.1%	15.3%	14.4%	14.0%	13.7%
VEHICLES AVAILABLE – U.S.									
No vehicles available	9.2%	9.4%	9.4%	8.9%	8.8%	8.7%	8.8%	8.9%	9.1%
1 vehicle available	33.6%	34.0%	33.8%	33.1%	33.2%	33.1%	33.4%	33.7%	33.8%
2 vehicles available	38.4%	38.3%	38.5%	38.2%	38.0%	38.1%	37.8%	37.6%	37.6%
3 or more vehicles available	18.9%	18.2%	18.3%	19.8%	20.0%	20.1%	20.0%	19.9%	19.5%
COMMUTING TO WORK – FLORIDA									
Car, truck, or van – drove alone	80.6%	81.3%	81.00%	80.0%	79.3%	79.6%	79.5%	79.3%	79.9%
Car, truck, or van – carpooled	10.7%	10.7%	10.10%	11.1%	10.9%	10.5%	10.3%	10.4%	9.6%
Public transportation (not taxi)	1.9%	1.8%	1.7%	1.8%	2.0%	1.9%	2.0%	1.9%	2.1%
Walked	1.5%	1.3%	1.5%	1.6%	1.7%	1.7%	1.5%	1.5%	1.7%
Other means	1.8%	1.5%	1.7%	2.0%	2.2%	2.1%	2.3%	2.2%	1.1%
Worked at home	3.5%	3.0%	4.0%	3.6%	4.0%	4.2%	4.5%	4.8%	4.6%
COMMUTING TO WORK – U.S.									
Car, truck, or van -- drove alone	77.4%	76.8%	76.3%	77.0%	76.0%	76.1%	75.5%	76.1%	76.6%
Car, truck, or van -- carpooled	10.4%	10.7%	11.2%	10.7%	10.7%	10.4%	10.7%	10.0%	9.7%
Public transportation (not taxi)	4.8%	4.9%	5.0%	4.7%	4.8%	4.9%	5.0%	5.0%	4.9%
Walked	2.5%	2.6%	2.7%	2.5%	2.9%	2.8%	2.8%	2.9%	2.8%
Other means	1.4%	1.5%	1.6%	1.6%	1.7%	1.7%	1.8%	1.7%	0.7%
Worked at home	3.5%	3.4%	3.2%	3.6%	3.9%	4.1%	4.1%	4.3%	4.3%
ZERO WORKER HOUSEHOLDS									
Florida	-	-	31.70%	31.6%	30.2%	30.0%	29.5%	31.6%	32.5%
U.S.	-	-	27.20%	27.0%	25.8%	25.8%	24.5%	26.3%	27.2%
MEAN TRAVEL TIME TO WORK									
Florida (mins)	24.8	24.8	25.4	26	25.9	25.9	25.9	25.4	25.5
U.S. (mins)	24.4	24.3	24.4	25.1	25	25.3	25.5	25.1	25.3



The work-at-home population has grown in the past decade across the nation and particularly in Florida. This is among the fastest changing aspects of commuting. Table 1 also includes the share of households that have no workers, which is 32.5% in Florida and 27.2% in the U.S. This is relevant to transportation policy in that the location and travel decisions of these households are not necessarily influenced by access to work considerations. This combined with work-at-home households results in more than one-third of Floridian households not involved in commuting.

Figure 1 contrasts Florida and the nation's zero-vehicle household trends. Vehicle availability (smaller share of zero-vehicle households) improved in Florida from 2002 through 2007. However, 2008 showed an increase in zero-vehicle households at both the state and national level and this trend continued through 2010. As zero-vehicle households are typically smaller - often single persons - the share of the population that resides in zero-vehicle households is quite small, 6.6% in the U.S. and 4.8% in Florida.

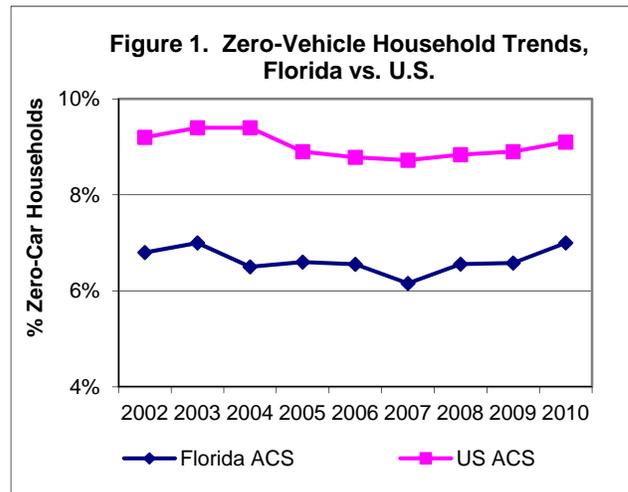


Figure 2 contrasts Florida and national trends with respect to carpool or shared-ride commuting and transit use. Reliance on driving or being a private-vehicle passenger remains the dominant means of commuting in Florida, with "drive alone" (Table 1) being higher than the rest of the country. Both public transit commuting and walking in Florida remain below the national average. Transit use remained the same in 2010 in Florida and decreased slightly in the U.S. Florida's declining carpooling trend is very similar to the U.S. trend.

In Figure 3, commute times rose for both Florida and the U.S., with Florida remaining above the national average each year. Florida's commute time has remained below its 2005 peak, while the national average is creeping back up to its 2008 peak. This measure does not allow discernment of the relative contributions of trip length changes versus trip speed changes.

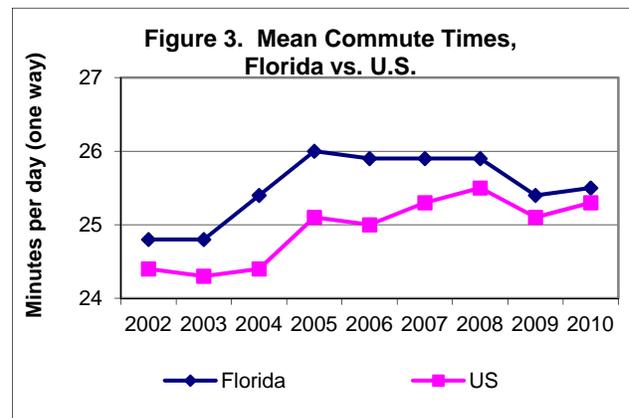
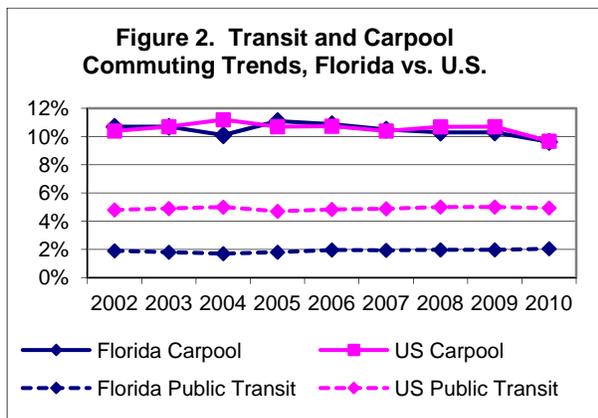




Table 2 shows the comparative transit mode shares for commuting for consolidated metropolitan statistical areas (CMSAs) and metropolitan statistical areas (MSAs). Transit use in all Florida CMSAs and MSAs is below the national average.

Table 3 provides the same data for counties. At the county level, only Miami-Dade County has a transit share that exceeds the national average.

Table 4 provides commute times for 15 Florida counties.

The county averages range from about 0.3 minutes longer than the national average to 6.3 minutes above the national average.

Table 2. Top Florida MSAs Ranked by Transit Mode Share to Work, 2010 ACS		
Rank	City – Area	Transit %
1	Gainesville, FL Metro Area	3.8
2	Miami–Fort Lauderdale–Pompano Beach, FL Metro Area	3.5
3	Orlando–Kissimmee, FL Metro Area	1.6
4	Cape Coral–Fort Myers, FL Metro Area	1.6
5	Naples–Marco Island, FL Metro Area	1.6
6	Tampa–St. Petersburg–Clearwater, FL Metro Area	1.5
7	Bradenton–Sarasota–North Port, FL Metro Area	1.1
8	Jacksonville, FL Metro Area	1.0
9	Deltona–Daytona Beach–Ormond Beach, FL Metro Area	1.0
10	Lakeland–Winter Haven, FL Metro Area	0.8
11	Tallahassee, FL Metro Area	0.8
12	Ocala, FL Metro Area	0.7
13	Punta Gorda, FL Metro Area	0.7
14	Port St. Lucie, FL Metro Area	0.5
15	Palm Bay–Melbourne–Titusville, FL Metro Area	0.5
16	Pensacola–Ferry Pass–Brent, FL Metro Area	0.5
17	Fort Walton Beach–Crestview–Destin, FL Metro Area	0.3
18	Sebastian–Vero Beach, FL Metro Area	0.1
	United States	4.9
	Florida	1.9

Table 3. Top Florida Counties Ranked by Transit Mode Share to Work, 2010 ACS (3-Yr Est.)		
Rank	County	Transit %
1	Miami-Dade	5.3
2	Alachua	3.4
3	Broward	2.9
4	Orange	2.5
5	Hillsborough	1.7
6	Monroe	1.6
7	Duval	1.6
8	Pinellas	1.6
9	Collier	1.5
10	Palm Beach	1.5
11	Pinellas	1.4
12	Osceola	1.4
13	Leon	1.2
	United States	5.0
	Florida	1.9

Note: Values in italics are not statistically significantly different than those ranked immediately above or below at the 95% confidence level.

Table 4. Top Florida Counties Ranked by Travel Time to Work, 2010 ACS			
Florida Rank	County	Minutes	US Rank
1	Hernando	31.6	51
2	Clay	31.0	60
3	Osceola	30.1	83
4	Pasco	29.5	95
5	Miami-Dade	28.9	111
6	Putnam	28.8	113
7	Nassau	28.3	134
8	St. Lucie	28.1	138
9	Citrus	27.7	150
10	Lake	27.1	179
11	Flagler	26.8	187
12	Broward	26.7	192
13	St. Johns	26.7	193
14	Santa Rosa	25.6	252
15	Seminole	25.6	253
	United States	25.3	
	Florida	25.5	



The top 5 Florida counties ranked by travel time to work from Table 4 also have the highest percentages of workers who work outside their county of residence. Table 5 provides selected rank data on cross-county commuting for selected states. Most states have more cross-county line commuting than Florida.

Figure 4 and Figure 5 provide comparisons across states in single occupant vehicle (SOV) shares and mean travel times. For SOV shares, Florida ranked in the middle when compared to those states shown in Figure 4. When it comes to mean commute time, only a dozen states had longer commute time than Florida as can be seen in Figure 5. In both cases, Florida was higher than the national average.

Table 5. Percent of Workers 16+ Years Who Worked Outside County of Residence, Selected States, 2010 ACS		
Rank	State	Percent
1	Virginia	51.3
2	Maryland	47
3	New Jersey	45.7
8	New York	35.2
11	Colorado	34.2
21	Wisconsin	28.3
22	North Carolina	28
	United States	27.4
25	Illinois	26.6
36	Texas	22.2
41	Florida	18.6
43	California	17
49	Arizona	6

